



THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN1
TITLE SHEET	1
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS	1A
STANDARD TRAFFIC DESIGN DRAWINGS	1A1
PROJECT COMMITMENTS	1B
ESTIMATED ROADWAY QUANTITIES	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B
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ENVIRONMENTAL NOTES.....	2E
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UTILITY NOTES AND UTILITY OWNERS.....	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL	T1

YEAR	PROJECT NO.	SHEET NO.
2025	NH/HSIP-32(104)	ROADWAY-SIGN1

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE SHEET

Index Of Sheets
SEE SHEET NO. 1A

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

CLAIBORNE COUNTY

SR-32 (US-25E): FROM NEAR YOUNG LANE TO NEAR SR-33

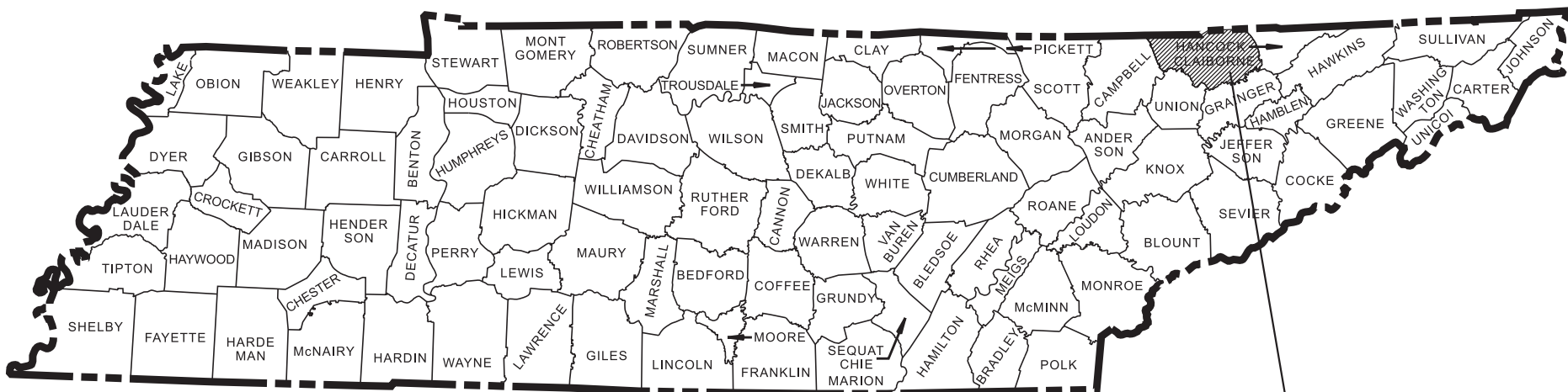
RESURFACE & SAFETY

THIN LIFT "D", PAVEMENT MARKINGS, GUARDRAIL

STATE HIGHWAY NO. 32 F.A.H.S. NO. 25E

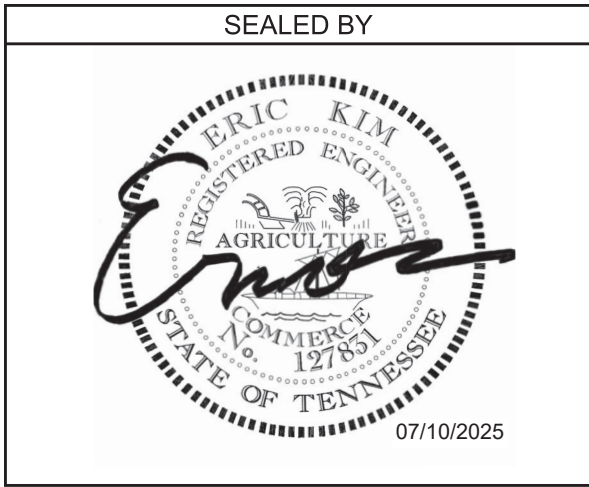
DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES	NO X

TENN.	YEAR	SHEET NO.
	2025	1
FED. AID PROJ. NO.	NH/HSIP-32(104)	
STATE PROJ. NO.	13S032-F3-006; 13S032-F8-006	



CLAIBORNE S.R. 32
BRIDGE ID. # 13SR0320007 13SR0320019 13SR0320025

NO EXCLUSIONS



APPROVED:
WILL REID, CHIEF ENGINEER

DATE:

APPROVED:
WILL REID, COMMISSIONER

TRAFFIC DATA	
ADT (2025)	9550
POSTED SPEED	
L.M. 4.16 - L.M. 7.70	55 MPH
L.M. 7.70 - L.M. 8.89	45 MPH

13S032-F8-006
13S032-F3-006
END PROJECT NO. NH/HSIP-32(104) RESURFACE & SAFETY
L.M. 8.89

13S032-F8-006
13S032-F3-006
BEGIN PROJECT NO. NH/HSIP-32(104) RESURFACE & SAFETY
L.M. 4.16

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT PROJECT MANAGER: ERIC WILSON, P.E.
DESIGNER : TYRIQUE MCSHAND CHECKED BY : AARON HAYS, P.E.
P.E. NO. 98013-4203-04 (DESIGN)
PIN NO. 134034.00



SCALE: 1"= 5280'

PROJECT LENGTH 4.73 MILES
TOTAL LANE MILES RESURFACED 23.65 MILES

ROADWAY INDEX

STANDARD ROADWAY DRAWINGS

SHEET NAME	SHEET NO.	DWG.	REV.	DESCRIPTION
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SIGNATURE SHEET	ROADWAY-SIGN1
TITLE SHEET	1
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*BRIDGE SHEETS.....	B-1

NOTE: THE ALPHABETICAL LETTERS “I”, “O” & “Q” ARE NOT USED IN THE NUMBERING OF SHEETS.

*PROVIDED BY TDOT STRUCTURES OFFICE

STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS

RD-TP-1	10-01-24	STANDARD ROADWAY DRAWINGS TITLE SHEET
RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND

STANDARDS ROADWAY DRAWINGS


RD11-SE-1		TRANSITION AND CROSS SLOPE DETAILS
RD11-SE-2		SUPERELEVATION TRANSITION DETAILS FOR UNDIVIDED ROADWAYS
RD11-SE-2A		SUPERELEVATION TRANSITION SECTIONS FOR UNDIVIDED ROADWAYS
RD11-LR-1		MINIMUM RUNOFF LENGTHS (LR) FOR URBAN HIGHWAYS
RD11-LR-2		MINIMUM RUNOFF LENGTHS (LR) FOR RURAL HIGHWAYS

SAFETY DESIGN AND GUARDRAILS

S-CZ-1	06-28-19	CLEAR ZONE CRITERIA
S-GRS-7	01-09-24	SHORT RADIUS-GR SYSTEM (SRGS) CONNECTION TO BRIDGE END
S-GRA-3	01-09-24	TYPE 13 GUARDRAIL ANCHOR
S-PL-2M		SAFETY PLAN AT SIDE ROADS OR PRIVATE DRIVES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-32(104)	1A

SEALED BY



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ROADWAY INDEX
AND
STANDARD
ROADWAY
DRAWINGS

STANDARD TRAFFIC DESIGN DRAWINGS

DWG. REV. DESCRIPTION

SIGNALS


T-SG-1	06-27-16	WOOD POLE DETAILS FOR SPAN MOUNTED SIGNALS
T-SG-2	06-27-16	LOOP LEAD-INS, CONDUIT AND PULL BOXES
T-SG-3	07-11-17	STANDARD NOTES AND DETAILS OF INDUCTIVE LOOPS

DESIGN - TRAFFIC CONTROL

T-M-1	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-M-3	01-24-25	MARKING STANDARDS FOR TRAFFIC ISLANDS, PAVED SHOULDERS AND MEDIANS FOR CONVENTIONAL ROADS
T-M-4	01-24-25	STANDARD INTERSECTION PAVEMENT MARKINGS
T-M-15A	01-24-25	ASPHALT SHOULDER RUMBLE STRIP INSTALLATION DETAILS FOR NON-ACCESS CONTROLLED RURAL ROUTES
T-M-16	01-24-25	RUMBLE STRIPE INSTALLATION LAYOUT
T-M-16A	01-24-25	RUMBLE STRIPE DETAILS FOR EDGE OF PAVEMENT AND CENTERLINE
T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-13	05-01-20	TWO-OUTSIDE LANE CLOSURE FOR EXPRESSWAY AND FREEWAYS
T-WZ-FAB1		FLASHING YELLOW ARROW BOARD

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-32(104)	1A1

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

STANDARD
TRAFFIC
DESIGN
DRAWINGS

PROJECT COMMITMENTS			
COMMITMENT ID	SOURCE DIVISON	DESCRIPTION	STA. / LOCATION
EDHZ001	ENVIRONMENT	AN ASBESTOS CONTAINING MATERIAL (ACM) SURVEY WAS COMPLETED ON BRIDGE NO. 13SR0320007 SR-32 OVER LITTLE SYCAMORE CREEK LM 4.41. NO ASBESTOS WAS DETECTED. PLEASE SEE THE REPORT FOR FURTHER DETAILS AND PHOTOGRAPHS. NO SPECIAL ACCOMMODATIONS FOR DEMOLITION AND WASTE DISPOSAL ARE ANTICIPATED FOR THESE STRUCTURES AND THE MATERIAL CAN BE DEPOSITED IN A C&D LANDFILL. PRIOR TO THE DEMOLITION OR REHABILITATION OF ANY STRUCTURE (BRIDGE OR BUILDING), THE CONTRACTOR IS REQUIRED TO SUBMIT THE NATIONAL EMISSION STANDARDS FOR HAZARDOUS AIR POLLUTANTS STANDARD 10-DAY NOTICE OF DEMOLITION TO THE TDEC DIVISION OF AIR POLLUTION CONTROL (PER TDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (JANUARY 1, 2021) SECTIONS 107.08.D AND 202.03).	BRIDGE ID. #13SR0320007

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-32(104)	1B

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07/10/2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PROJECT
COMMITMENTS


ESTIMATED ROADWAY QUANTITIES					
ITEM NO.	DESCRIPTION	UNIT	NH/HSIP-32(104) QUANTITY 13S032-F8-006	NH/HSIP-32(104) QUANTITY 13S032-F3-006	TOTAL QUANTITY
	208-01.05	BROOMING & DEGRASSING SHOULDERS	L.M.	10	10
(1)	307-01.15	ASC MIX (PG64-22) (BPMLC-HM) GRADING CS	TON	2053	2053
	403-01	BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	165	165
(2)	411-03.13	ACS MIX(PG70-22) THIN LIFT D ASPHALT	TON	10262	10262
(6)	411-12.02	SCORING SHOULDERS (NON-CONTINUOUS) (16IN WIDTH)	L.M.	7	7
(7)	415-01.02	COLD PLANING BITUMINOUS PAVEMENT	S.Y.	384	384
(10)	705-06.11	GR TERMINAL (IN-INLINE) MASH TL3	EACH	1	1
(10)	706-01	GUARDRAIL REMOVED	L.F.	85	85
(10)	706-06.03	RADIUS RAIL	L.F.	85	85
	712-01	TRAFFIC CONTROL	LS	1	1
	712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	142	142
	712-05.01	WARNING LIGHTS (TYPE A)	EACH	71	71
(3)	712-06	SIGNS (CONSTRUCTION)	S.F.	237	237
	712-08.03	ARROW BOARD (TYPE C)	EACH	2	2
	716-01.21	SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR) (1 COLOR)	EACH	834	834
	716-01.22	SNOWPLOWABLE RAISED PAVMENT MARKERS (MONO-DIR)(1 COLOR)	EACH	626	626
(4)	716-01.30	REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH	1460	1460
(8)(9)	716-02.04	PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING)	S.Y.	95	95
(8)	716-02.05	PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	480	480
(8)	716-02.06	PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	50	50
(8)	716-02.08	PLASTIC PAVEMENT MARKING (8" DOTTED LINE)	L.F.	265	265
(8)	716-02.12	PLASTIC PAVEMENT MARKING (6IN LINE)	L.M.	0.1	0.1
(8)	716-04.04	PLASTIC PAVEMENT MARKING (TRANSVERSE SHOULDER)	L.F.	240	240
(5)	716-05.20	PAINTED PAVEMENT MARKING (6" LINE)	L.M.	9.6	9.6
	716-13.02	SPRAY THERMO PVMT MRKNG (60 mil) (6IN LINE)	L.M.	24	24
	717-01	MOBILIZATION	LS	1	1
	730-14.02	SAW SLOT	L.F.	1500	1500
	730-14.03	LOOP WIRE	L.F.	3000	3000

FOOTNOTES	
(1)	TO BE USED FOR SPOT LEVELING. SPOT LEVELING TO BE USED ONLY TO CORRECT DEFICIENCIES ON EXISTING PAVEMENT AND IS NOT TO BE COMBINED WITH OTHER COURSES.
(2)	INCLUDES <u>19 TONS</u> FOR THE RIGHT TURN LANE AT TOM BELL RD. INTERSECTION (L.M. 7.72), AND <u>90 TONS</u> FOR COUNTY ROADS. BUSINESS ENTRANCES AND PRIVATE DRIVES ARE TO BE TIED-IN AS NEEDED OR AS DIRECTED BY THE TDOT MANAGER.
(3)	SEE SHEET 2F FOR SIGN TABULATION. THE CONSTRUCTION SIGNING IS TO BE A MINIMUM. OTHER SIGNS MAY BE REQUIRED AS DIRECTED BY THE TDOT MANAGER.
(4)	ANY DAMAGE OCCURRED DURING REMOVAL SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE AND TO THE SATISFACTION OF THE TDOT MANAGER.
(5)	TO BE USED FOR TEMPORARY STRIPING DURING PLACEMENT OF SPOT LEVELING ONLY. NO PAYMENT WILL BE MADE FOR TEMPORARY STRIPING DURING PLACEMENT OF THE FINAL SURFACE (ON EITHER EXISTING SURFACE OR FINAL) COURSE DUE TO THE CONTRACTOR'S METHODS OF CONSTRUCTION.
(6)	TO BE PLACED FROM L.M. 4.16 TO L.M. 8.03. QUANTITY CALCULATED BY MULTIPLYING THE PROJECT LENGTH BY A FACTOR OF (60/75).
(7)	TO BE USED AT THE BEGINNING AND END OF PROJECT LIMITS AND FOR BRIDGE END TRANSITIONS AT L.M. 4.41 AND L.M. 6.61.
(8)	THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
(9)	TO BE USED FOR 12" CHEVRON MARKINGS AND 8" CHANNELIZING LINE OF MARKED ISLAND.
(10)	TO BE PLACED AT L.M. 7.73.

NOTE: THERE ARE NO UTILITY ADJUSTMENTS ON THIS PROJECT.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-32(104)	2

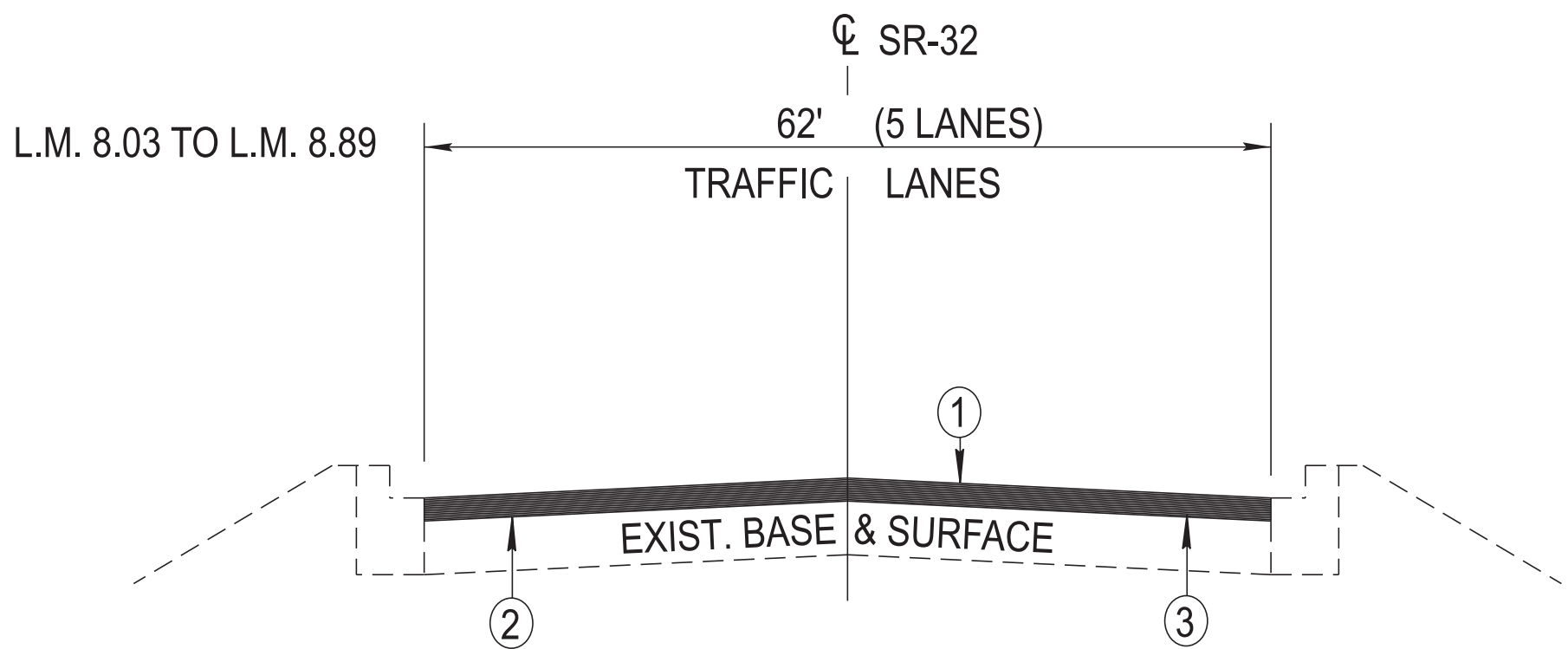
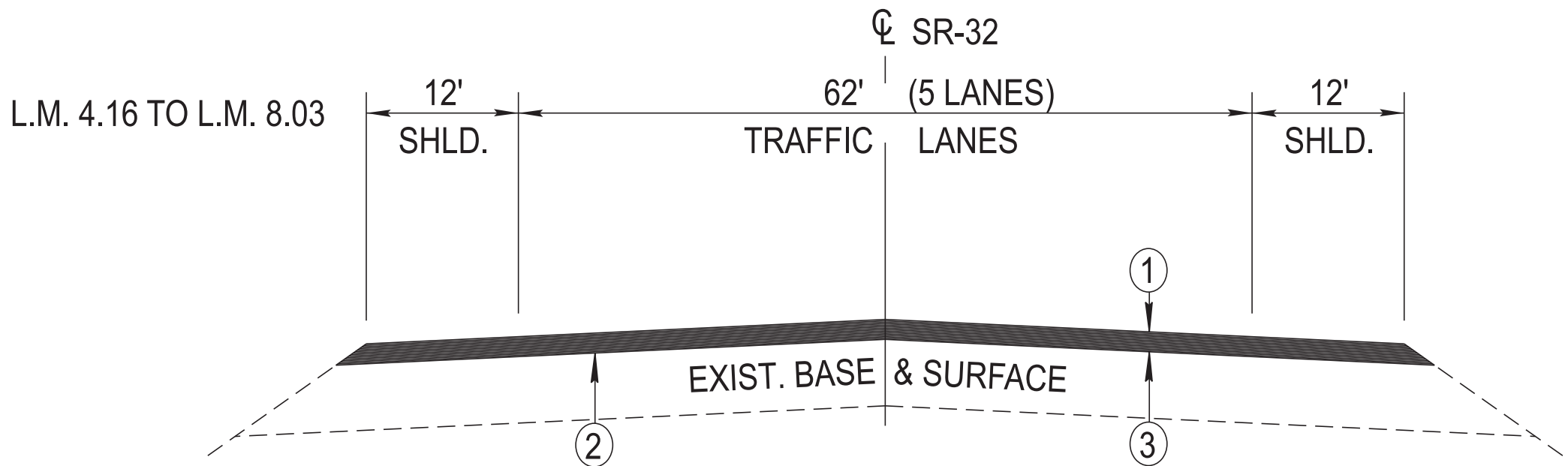
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED
ROADWAY
QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-32(104)	2B



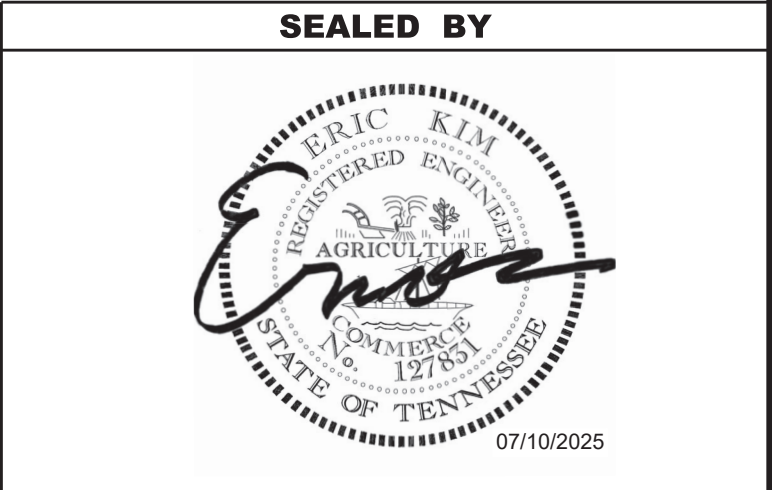
PROPOSED PAVEMENT SCHEDULE	
①	THIN LIFT ASPHALT (HM) ¾" (APPROX. 85 LBS/SY) ITEM NO. 411-03.13 ACS MIX (PG70-22) THIN LIFT D ASPHALT
②	BITUMINOUS PLANT MIX BASE (HM) (SPOT LEVELING) ITEM NO. 307-01.15 ACS MIX (PG64-22) (BPMLC-HM) GRADING CS
③	TACK COAT (TC) (APPROX. 0.15 GAL/SY) ITEM NO. 403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC)
	COLD PLANING ¾" TH. (APPROX. 78.8 LBS/SY) ITEM NO. 415-01.02 COLD PLANING BITUMINOUS PAVEMENT

BRIDGE NOTES:

BRIDGE OVER LITTLE SYCAMORE CREEK (L.M. 4.41) IS TO BE SEALED WITH TYPE 1 THIN EPOXY OVERLAY AND BRIDGE JOINTS REPLACED (SEE BRIDGE SHEETS).

BRIDGE OVER OLD U.S. 25E (L.M. 6.61) IS TO HAVE THE BRIDGE JOINTS REPLACED (SEE BRIDGE SHEETS).

BRIDGE OVER RUSSEL CREEK (L.M. 8.78) IS TO BE PAVED WITH ¾" 411-03.13 ACS MIX (PG70-22) THIN LIFT D ASPHALT.



STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION
TYPICAL SECTIONS AND PAVEMENT SCHEDULE

GENERAL NOTES

GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

GUARDRAIL

- (1) THE CONTRACTOR SHALL NOT REMOVE ANY SECTIONS OF EXISTING GUARDRAIL TO REWORK SHOULDERS OR FLATTEN SLOPES UNTIL THE ENGINEER CONCURS IN THE NECESSITY OF REMOVAL DUE TO CONSTRUCTION REQUIREMENTS AND THE APPROPRIATE WARNING DEVICES ARE INSTALLED. THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.

MISCELLANEOUS

- (2) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES AND POSTS WHERE AND AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN PRICE BID FOR OTHER CONSTRUCTION ITEMS.
- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

- (2) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20, PAINTED PAVEMENT MARKING (6” LINE), L.M.

FINAL PAVEMENT MARKING

- (6) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:

a. BROOMING & DE-GRASSING SHOULDERS SHALL INCLUDE CLIPPING OF MATERIAL INTERFERING WITH PROPER DRAINAGE OF ROADWAY (INCLUDING PAVED AND GRAVEL SHOULDERS), AS DIRECTED BY THE TDOT PROJECT ENGINEER.

b. ALL MATERIAL FROM CLIPPING, BROOMING AND DE-GRASSING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE TDOT PROJECT ENGINEER.

c. ALL COSTS ASSOCIATED WITH PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.

d. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (11) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6” SPRAY THERMOPLASTIC (60 mil) INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-13.02, SPRAY THERMO PVMT MRKNG (60 mil) (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

SNOWPLOWABLE REFLECTIVE PAVEMENT MARKERS

- (19) REMOVE EXISTING SNOWPLOWABLE MARKERS PRIOR TO PAVING AND/OR COLD PLANING. REMOVE ALL ADHESIVES PRIOR TO PAVING. PATCH ANY HOLES OR DIVOTS RESULTING FROM THE REMOVAL OF A MARKER IN A MANNER WHICH ENSURES A UNIFORM PAVED SURFACE. PATCH WORK SHALL BE INCLUDED WITH COST OF OTHER ITEMS OF CONSTRUCTION.

PAVEMENT

PAVING

- (2) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

- (4) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (5) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (6) PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVER WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED, IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVER WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVER WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVER WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVER WIDTH.
- (7) ON CURB AND GUTTER SECTIONS, PUBLIC ROAD INTERSECTIONS SHALL BE RESURFACED TO THE END OF RADIUS. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD SHALL BE PROVIDED.
- (8) ON URBAN TYPICAL SECTIONS, (CURB AND GUTTER), RESIDENTIAL DRIVEWAYS AND BUSINESS ENTRANCES SHALL HAVE A MINIMUM WIDTH OF MATERIAL NOT LESS THAN ONE FOOT USED IN THE TRANSITION TO FEATHER THE PAVEMENT EDGE.

SIGNING

- (12) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.

TRAFFIC CONTROL DIRECTIONAL SIGNING

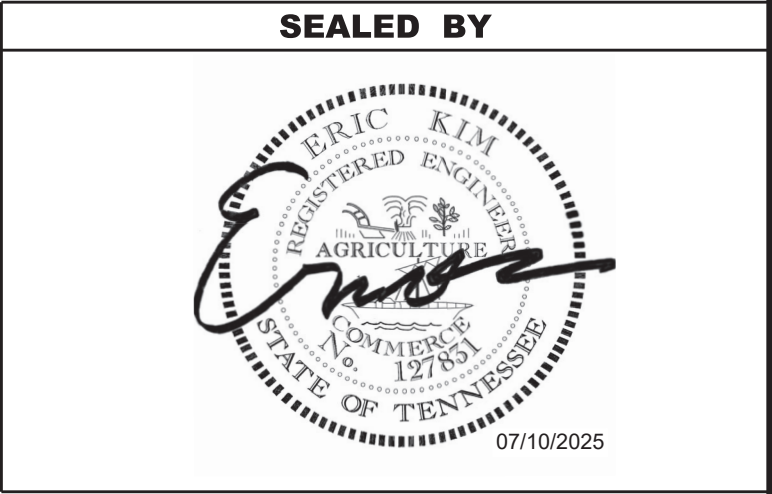
- (7) WHEN EXISTING “TOURIST ORIENTED DIRECTIONAL SIGNS” (TODS) ARE ON NON-ACCESS CONTROLLED CONSTRUCTION PROJECTS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THESE SIGNS IN FULL VIEW TO THE MOTORING PUBLIC DURING ALL PHASES OF CONSTRUCTION. ALL WORK IN MOVING THESE “TODS” AND TEMPORARY SUPPORTS ARE TO BE PAID FOR UNDER ITEM NO. 712-06, AS DIRECTED BY THE ENGINEER. NEW SUPPORTS AND SIGN FACE FOR FINAL LOCATION WILL BE PAID FOR UNDER OTHER ITEMS OF CONSTRUCTION.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.

- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER’S APPROVAL TO USE THEM.
- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER’S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSSIP-32(104)	2C



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GENERAL
NOTES

SPECIAL NOTES

DEMOLITION

DEMOLITION, REPAIR, OR REHABILITATION OF BRIDGES

- (1)

THE CONTRACTOR SHALL VERIFY THAT AN ASBESTOS SURVEY HAS BEEN COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATIONS ACTIVITIES (NOT INCLUDING ASPHALT MILLING OR OVERLAY).
- (2)

ASBESTOS-CONTAINING MATERIALS (ACM) ABATEMENT IS THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATION OF BRIDGE(S). ABATEMENT SHOULD BE ACCOMPLISHED PER SP202ACM SPECIAL PROVISION REGARDING REMOVAL OF ASBESTOS-CONTAINING MATERIALS. STATE OF TENNESSEE ASBESTOS ACCREDITATION REQUIREMENTS (TCA 1200-01-20) MANDATE THAT ACM ABATEMENT WORK BE PERFORMED BY AN ACCREDITED FIRM (CONTRACTOR) USING ACCREDITED ABATEMENT WORKERS AND SUPERVISORS.
- (3)

THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUBMITTING A NOTICE TO THE TDEC, DIVISION OF AIR POLLUTION CONTROL TEN (10) DAYS IN ADVANCE OF ANY ACM ABATEMENT, DEMOLITION, OR MAJOR REPAIR INVOLVING THE REMOVAL/REPLACEMENT OF A STRUCTURAL COMPONENT.

MISCELLANEOUS

- (1)

ALL GUARDRAIL, GUARDRAIL POSTS AND SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE REMOVED AND STORED BY STATE FORCES.
- (2)

ALL SIGNING AND PAVEMENT MARKINGS ARE TO BE IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

PAVEMENT

RESURFACING

- (2)

THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:

A.

BROOMING & DEGRASSING SHALL INCLUDE NOTCHING THE GRAVEL SHOULDER PRIOR TO MILLING. THE NOTCH SHALL BE 1.5 IN DEEP AND 2 FT WIDE, OR AS DIRECTED BY THE TDOT PROJECT ENGINEER.

B.

ALL MATERIAL FROM NOTCHING AND BROOMING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE TDOT PROJECT ENGINEER.

C.

ALL COSTS ASSOCIATED WITH NOTCHING, PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.

(3)

SURFACE IS TO BE CROWNED AS DIRECTED BY THE TDOT MANAGER.

(4)

THE CONTRACTOR WILL PLACE THE SPOT LEVELING WHERE AND AS DIRECTED BY THE TDOT MANAGER.

(5)

ALL SURPLUS PATCHING, JOINT MATERIAL AND CRACK SEALANT IS TO BE REMOVED BEFORE PLACING THE FIRST LAYER OF ASPHALT.

(6)

STATE FORCES WILL REMOVE ALL RAISED PAVEMENT MARKERS, IF ANY, BEFORE RESURFACING IS BEGUN.

(7)

COLD PLANING IS TO BE DONE IMMEDIATELY PRIOR TO THE COMMENCEMENT OF PAVING OPERATIONS. COLD PLANED SURFACES ARE NOT TO BE LEFT EXPOSED FOR MORE THAN FIVE (5) CALENDAR DAYS BEFORE BEING OVERLAYED BY THE FIRST COURSE OF ASPHALT. IF NECESSARY, COLD PLANING OPERATIONS ARE TO BE SUSPENDED TO ALLOW PAVING OPERATIONS TO CATCH UP. AFTER COLD PLANING AND PRIOR TO ALLOWING USE BY TRAFFIC, THE CONTRACTOR WILL RAMP ASPHALT AROUND EXISTING UTILITIES TO ELIMINATE HAZARD TO VEHICLES. ALL COLD PLANING REQUIRED ON CITY STREETS OR SIDE ROADS WILL BE PAID FOR AT THE UNIT PRICE BID FOR ITEM NO. 415-01.02.

SIGNALIZATION


- (1)

LOOP REPLACEMENT SHALL BE IN ACCORDANCE WITH THE TDOT STANDARD SPECIFICATIONS, SECTION 730.
- (3)

SIGNAL DETECTION LOOPS SHALL BE INSTALLED BEFORE THE FINAL SURFACE IS APPLIED.

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SPECIAL
NOTES

ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (4) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES


- (11) SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12) IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

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RESURF.	2025	NH/HSIP-32(104)	2E

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL
NOTES

EROSION PREVENTION AND SEDIMENT CONTROL NOTES

DISTURBED AREA

- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

SEDIMENT CONTROL

- (6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.

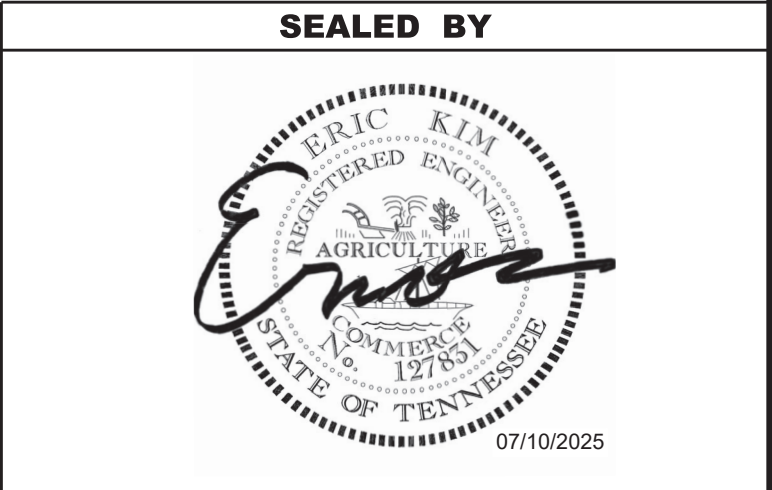
- (35) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.

- (52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION


EROSION
PREVENTION
AND SEDIMENT
CONTROL NOTES

TRAFFIC CONTROL SIGN TABULATION (RESURFACING)						
M.U.T.C.D. SIGN NO.	LEGEND \ DESCRIPTION	SIZE IN INCHES			TOTAL NUMBER REQUIRED	ITEM NO. 712-06 S.F.
		L	x	W		
W20-1	ROAD WORK	48"	x	48"	16	96
W20-5	LANE CLOSED	48"	x	48"	16	32
W4-2	LANE ENDS	48"	x	48"	16	32
G20-1	ROAD WORK NEXT 5 MILES	48"	x	24"	8	16
W13-1P	ADVISORY SPEED	30"	x	30"	6	13
G20-2	END ROAD WORK	48"	x	24"	8	16
W8-11	UNEVEN LANES	48"	x	48"	16	32
					TOTAL	237

NOTE: THE CONSTRUCTION SIGNING IS TO BE A MINIMUM. OTHER SIGNS
MAY BE REQUIRED AS DIRECTED BY THE TDOT MANAGER.

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TABULATED
QUANTITIES

UTILITY

- (2)

UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3)

THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4)

PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5)

THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS

ELECTRIC:
POWELL VALLEY ELECTRIC CO-OP
P.O. BOX 1528
420 STRAIGHT CREEK ROAD
NEW TAZWELL, TN 37825
PHONE: (423)626-5204
FAX: (423)626-0711
CONTACT: BO GOODIN
CELL PHONE: (606)545-2972
EMAIL: BGOODIN@PVE.COOP
CONTACT: BRAD COPPOCK
CELL PHONE: (423)626-0728
EMAIL: BCOPPOCK@PVE.COOP
CONTACT: JOHN HOYLE
CELL PHONE: (423)626-0716
EMAIL: JHOYLE@PVE.COOP


WATER/SEWER/GAS:
CLAIBORNE UTILITY DISTRICT
P.O. BOX 606(37825)
630 DAVIS DRIVE
NEW TAZWELL, TN 37824
PHONE: (423)626-4282
FAX: (423)626-3972
CONTACT: GEORGE COOTS
CELL PHONE: (423)526-7329
EMAIL: GCOOTS@CUDTN.COM

CABLE:
XFINITY
5720 ASHEVILLE HWY
KNOXVILLE, TN 37924
PHONE: (865)862-5061
FAX: (865)862-5098
CONTACT: MICHAEL BAILEY
CELL PHONE: (865)320-2675
EMAIL: MICHAEL_BAILEY2@CABLE.COMCAST.COM

TELEPHONE:
AT&T
9733 PARKSIDE DR.
KNOXVILLE, TN 37922
CONTACT: JAY FRAZIER
CELL PHONE: (865)387-2685
EMAIL: JF092G@ATT.COM

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07/10/2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

UTILITY NOTES
AND
UTILITY OWNERS

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:
- a.

WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
- b.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
- c.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
- d.

WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
2.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.
- a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
- (1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
- (2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
- b.

IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
- c.

WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE REGIONAL TRAFFIC ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

3.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 6 INCHES BUT NOT EXCEEDING 18 INCHES, THE CONTRACTOR, WITH THE ENGINEER’S APPROVAL, MAY UTILIZE ONE OF THE FOLLOWING:

- a.

THE CONTRACTOR SHALL ACCOMPLISH SEPARATION BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
- (1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
- (2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

IN ORDER TO USE THIS METHOD, THE CONTRACTOR MUST REDUCE THE DIFFERENCE IN ELEVATION TO 6 INCHES OR LESS BY THE END OF THE WORKDAY THAT THE CONDITION IS CREATED.

- b.

THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a, AND CONSTRUCT A STONE WEDGE WITH A 4:1 SLOPE, OR FLATTER, TO ELIMINATE THE VERTICAL OFFSET IF THE LOWER ELEVATION IS AT OR BELOW SUBGRADE AT THE END OF EACH DAY.
- c.

THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a AND IF THE LOWER ELEVATION IS BASE STONE OR ASPHALT PAVEMENT, PLACEMENT OF SUBSEQUENT LAYERS OF PAVEMENT MUST BEGIN THE NEXT WORK DAY AND PROGRESS CONTINUOUSLY UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED OR REDUCED TO SIX INCHES OR LESS.
- d.

THE CONTRACTOR SHALL PROVIDE SEPARATION BY PORTABLE BARRIER RAIL.

FOR PRECEDING CONDITIONS a, b, AND c, THE CONTRACTOR SHALL USE THE SHOULDER DROP-OFF WARNING SIGN WITH PLAQUE (W8-17 AND W8-17P). IT SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN THE SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

4.

FOR DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 18 INCHES.

SEPARATION WILL BE PROVIDED BY USE OF PORTABLE BARRIER RAIL.

IN THIS SITUATION THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

B. IF THE DIFFERENCE IN ELEVATION IS WITHIN 30 FEET OF THE NEAREST TRAFFIC LANE BEING USED BY TRAFFIC CAUSED BY GRADING, EXCAVATION FOR UTILITIES, DRAINAGE STRUCTURES, UNDERCUTTING, ETC.:

1.

IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 0.75 INCH AND NOT EXCEEDING 2 INCHES.
- a.

WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
2.

IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES:
- a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
- (1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
- (2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
3.

IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 6 INCHES:
- a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
- (1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
- (2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
- b.

ELIMINATE VERTICAL OFFSET BY CONSTRUCTING A STONE WEDGE OR GRADING TO A 4:1 SLOPE, OR FLATTER, OR USE PORTABLE BARRIER RAIL.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE WITHIN 8 FEET OF A TRAFFIC LANE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

C. IF THE DIFFERENCE IN ELEVATION IS FARTHER THAN 8 FEET FROM THE NEAREST TRAFFIC LANE BUT NOT MORE THAN 30 FEET FROM THE NEAREST TRAFFIC LANE:

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

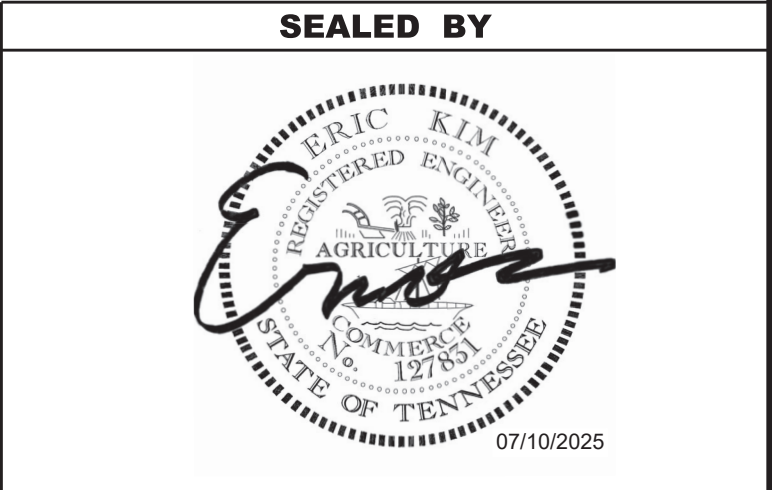
1.

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
2.

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	NH/HSIP-32(104)	T1



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT EDGE
DROP-OFF NOTES
FOR
TRAFFIC CONTROL

FEDERAL PROJECT # NH/HSIP-32(104)			
PROJECT NO.		YEAR	SHEET NO.
13S032-M3-009		2025	B-1
REVISIONS			
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INDEX OF DRAWINGS

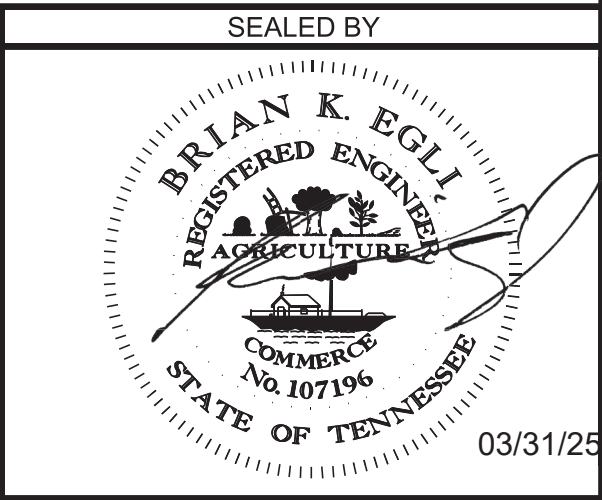
DWG. NO.

SIGNATURE SHEET	STRUCTURE-SIGN 1
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
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13-SR32-4.41 OVER
LITTLE SYCAMORE CREEK
AND 13-SR32-6.61 OVER
OLD SR-32
CLAIBORNE COUNTY
2025

PIN NO.:134034.00

DESIGN BY: SILESHI ERGICHO

DRAWN BY: SILESHI ERGICHO

SUPERVISED BY: KEVIN MARTINKO

CHECKED BY:

DATE: / /

DATE: 2/25

DATE: 2/25

DATE: / /

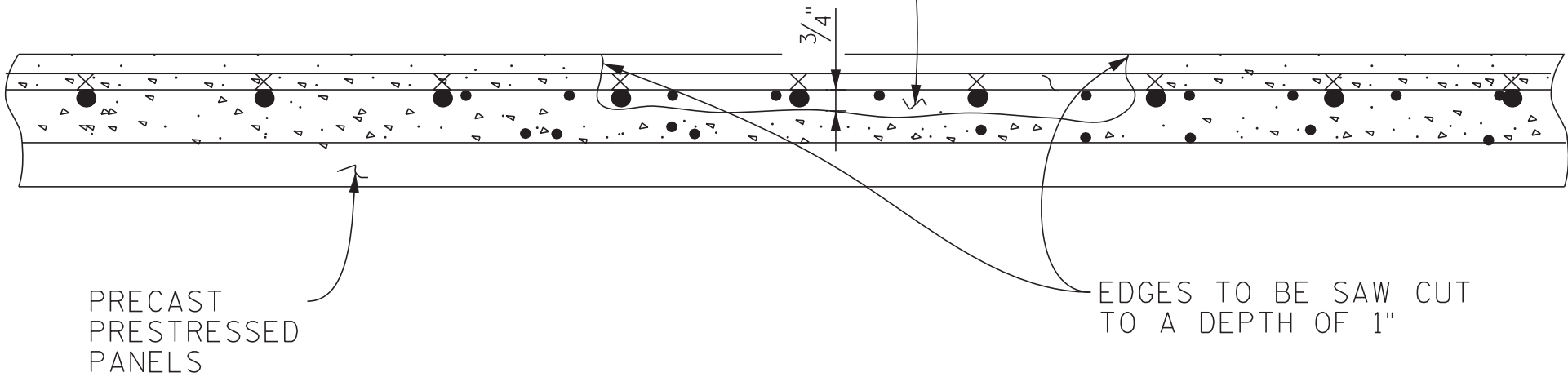
PROJECT NO.		YEAR	SHEET NO.
13S032-M3-009		2025	B-2
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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POWER DRIVEN HAND TOOLS:

POWER DRIVEN HAND TOOLS USED FOR THE REMOVAL OF UNSOUND CONCRETE IN MAKING PARTIAL AND FULL DEPTH REPAIRS ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:
(1) PARTIAL DEPTH REPAIRS; PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 60 LB. CLASS SHALL NOT BE USED. ALSO TRAFFIC CONTROL SHALL BE SET UP DURING PARTIAL DEPTH REPAIRS OVER TRAFFIC.
(2) FULL DEPTH REPAIRS; PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 90 LB. CLASS SHALL NOT BE USED. ALSO ALL DECK REPAIR OVER BEAMS WILL BE RESTRICTED TO 60 LB. PNEUMATIC HAMMERS.
(3) CHIPPING HAMMERS OF THE 15 LB. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL.

TABULATION OF BRIDGE RELATED WORK AND ESTIMATED QUANTITIES						
LOCATION OF BRIDGE AND BRIDGE NUMBER	REFERENCE DRAWINGS TO BE PRINTED WITH CONTRACT DRAWINGS	TYPE OF WORK	604-10.44 EXPANSION JOINT REPAIRS L.F.	604-10.50 BRIDGE DECK REPAIRS (PARTIAL DEPTH OF SLAB) S.Y.	604-10.53 COCRETE SLAB REPAIRS (PARTIAL DEPTH OF APPROACH PAVEMENT) S.Y.	617-04.01 TYPE 1 THIN EPOXY OVERLAY (EPOXY-URETHANE) S.Y.
13-SR32-4.41 OVER LITTLE SYCAMORE CREEK (13SR0320007)	M-431-1 M-431-4 STD-1-5	EXPANSION JOINT REPAIRS, CONCRETE REPAIRS, BRIDGE DECK REPAIRS AND TYPE 1 THIN EPOXY OVERLAY	195	5	5	2535
13-SR32-6.61 OVER OLD SR-32 (13SR0320025)	M-431-23 M-431-26 STD-1-5	EXPANSION JOINT REPAIRS	349			
	TOTAL		544	5	5	2535

RAPID SET CEMENTITIOUS PATCHING MATERIALS (FOR PATCHING MATERIAL REFER TDOT QUALIFIED PRODUCT LIST NO.13.004)



SKETCH SHOWING DECK REPAIR

NOTE:PARTIAL DEPTH DECK REPAIR ONLY REMOVE CONCRETE IN ALL DELAMINATED AREAS TO A DEPTH OF 3/4" BELOW THE TOP BAR OF THE TOP MAT OF REINFORCING STEEL.

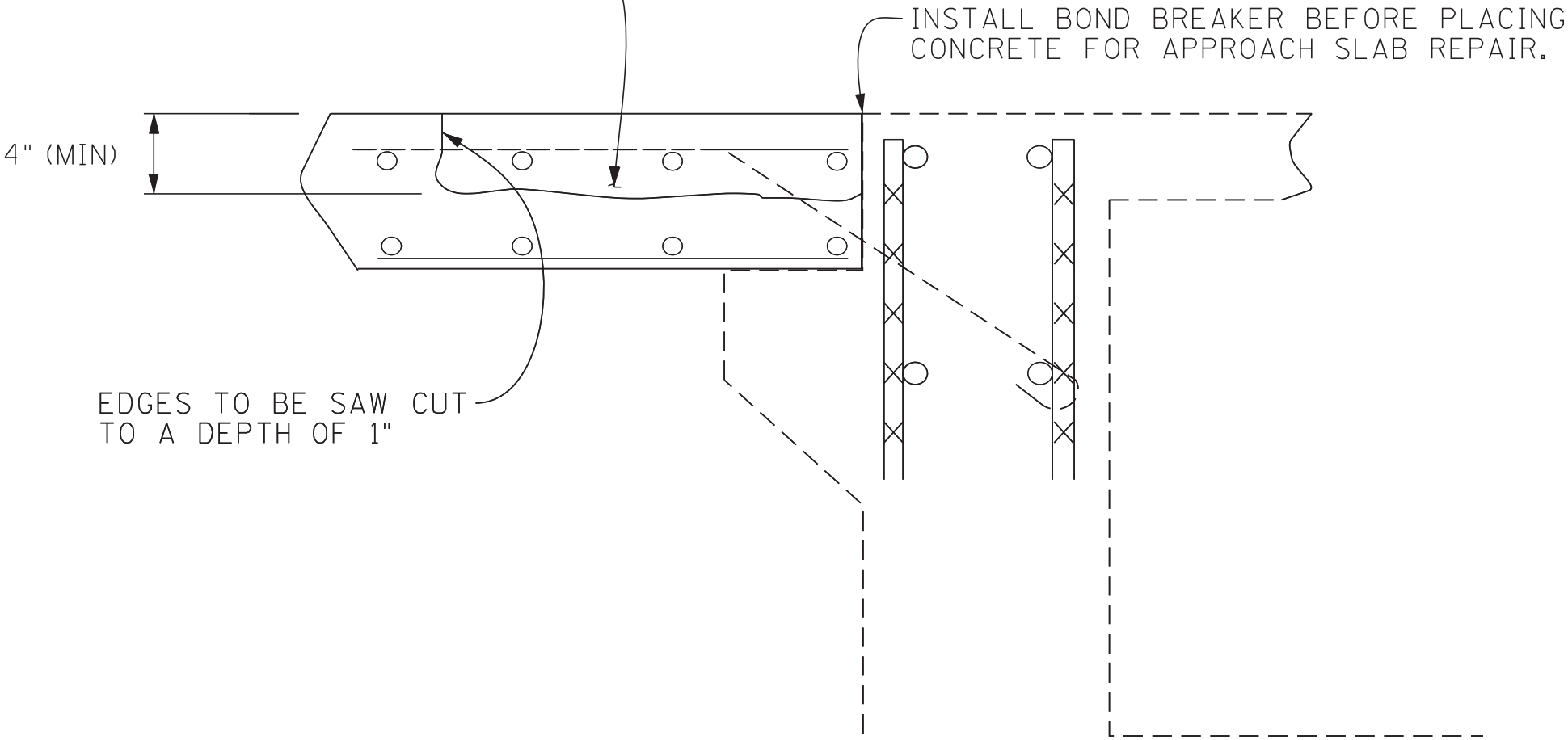
NOTE:PARTIAL DEPTH DECK REPAIRS ALL REINFORCING STEEL IN THE AREAS OF THE DECK REPAIRS SHALL BE COMPLETELY CLEANED. CLEANING SHALL BE DONE PRIOR TO PLACING NEW CONCRETE. AREAS OF CONCRETE REMOVAL SHALL BE DESIGNATED BY PERSONNEL FROM THE HEADQUARTERS, BRIDGE INSPECTION AND REPAIR OFFICE. INSPECTIONS TO DETERMINE AREAS OF DECK REPAIR SHALL BE SCHEDULED WITH THE BRIDGE REPAIR OFFICE AT LEAST (3) DAYS IN ADVANCE.

THE COST OF CONCRETE, CONCRETE REMOVAL, LABOR, CLEANING REBAR AND ANY MISCELLANEOUS MATERIAL REQUIRED SHALL BE PAID FOR UNDER ITEM NO. 604-10.50 BRIDGE DECK REPAIRS (PARTIAL DEPTH OF SLAB), S.Y.

NOTE: ITEM NO. 604-10.50, BRIDGE DECK REPAIRS (PARTIAL DEPTH OF SLAB) MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.

NOTE: THE CONTRACTOR SHOULD TAKE EXTREME CARE WHEN REMOVING THE CONCRETE FOR PARTIAL DEPTH OF DECK REPAIR, SO AS NOT DAMAGE THE EXISTING PRECAST PRESTRESSED PANELS. IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL BE RESPONSIBLE FOR NECESSARY REPAIRS ON ALL DAMGED PANNELS TO THE SATISFACTION OF THE PROJECT ENGINEER AT NO ADDITIONAL COST.

SEE BRIDGE DECK REPAIR MATERIAL NOTE BELOW



CONCRETE APPROACH PAVEMENT REPAIR DETAILS:

NOTE: REMOVE CONCRETE TO A DEPTH OF 3/4" MINIMUM BELOW EXISTING REINFORCING STEEL OR TO SOUND CONCRETE. CONCRETE REMOVAL SHOULD BE TO A MINIMUM DEPTH OF 4". DEPTH FOR CONCRETE REMOVAL SHOULD NOT BE MORE THAN 6".

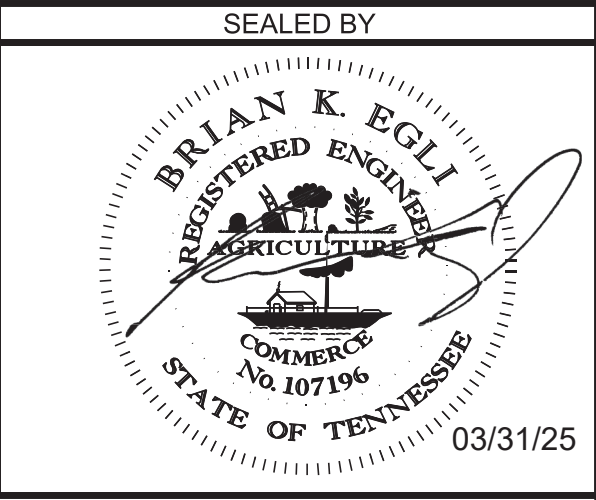
NOTE: COST OF SAW CUTTING, CONCRETE REMOVAL, COMPLETELY CLEANING EXPOSED REINFORCING STEEL, LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO COMPLETE THE REPAIRS AS SHOWN SHALL BE INCLUDED IN ITEM NO. 604-10.53 CONCRETE REPAIR S.Y.

NOTE: ITEM NO. 604-10.53, CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PVMT.) MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.

BRIDGE DECK REPAIR MATERIAL

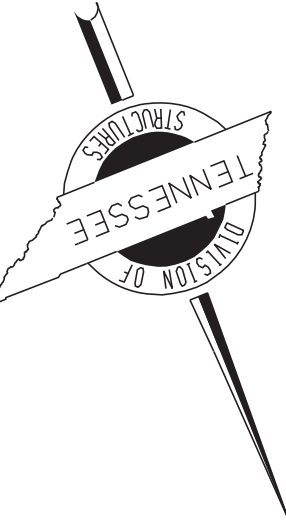
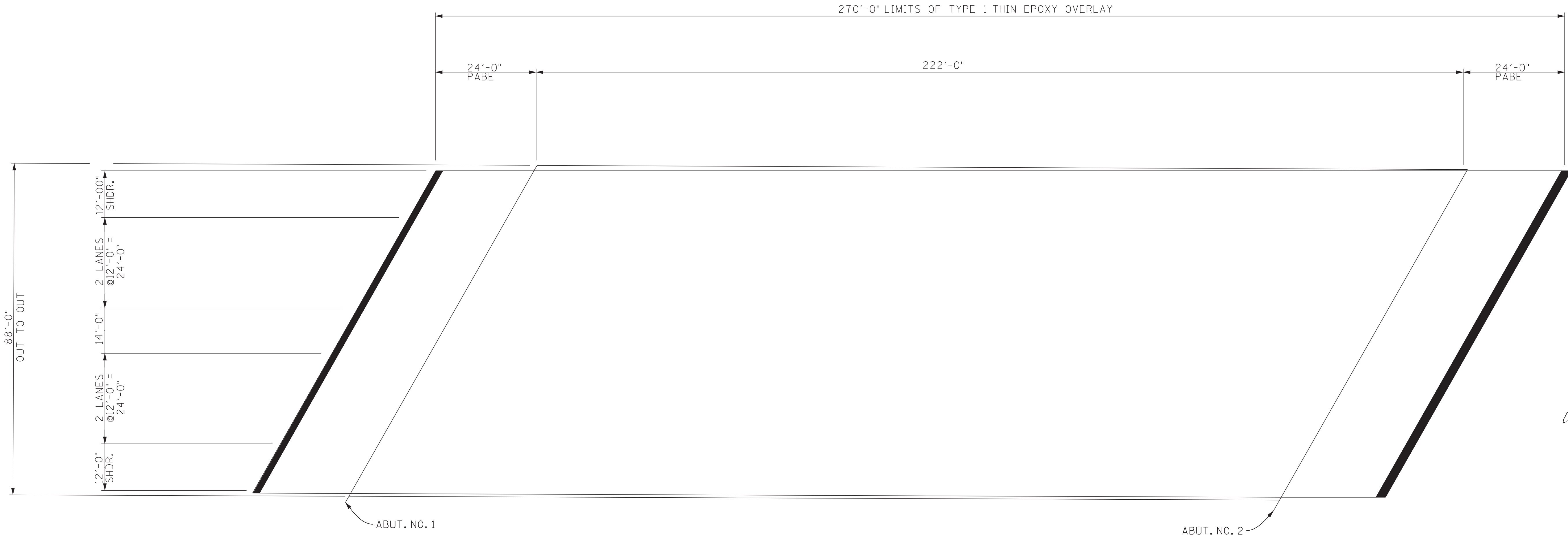
ALL REPAIRS SHALL USE ONLY THE NON-MAGNESIUM PHOSPHATE QUICK SET PATCHING MATERIAL FROM THE TDOT QUALIFIED PRODUCTS LIST: 13.004-RAPID SET CEMENTITIUS PATCHING MATERIALS. SHALL BE APPLIED ACCORDING TO MANUFACTURER'S SPECIFICATIONS.

PATCHING MATERIAL SHOULD BE ABLE TO ACHIEVE A MINIMUM 3000PSI AT 18 HOURS. PATCHING MATERIAL SHALL REACH 3000PSI BEFORE OPENING TO THE TRAFFIC.



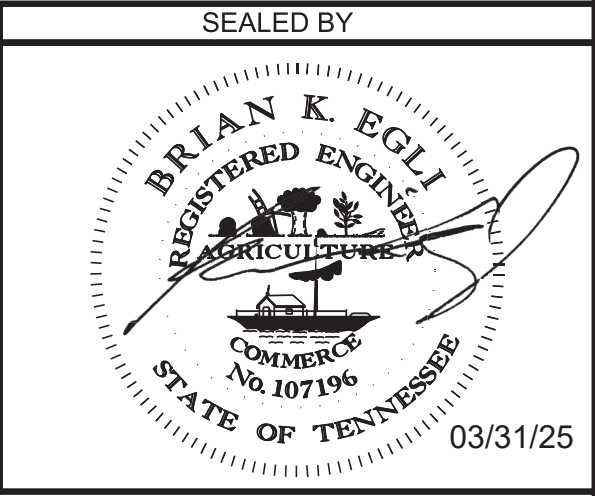
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BRIDGE TABULATION,
ESTIMATED QUANTITIES,
REPAIRS NOTES AND DETAILS
13-SR32-4.41 OVER
LITTLE SYCAMORE CREEK
AND 13-SR32-6.61 OVER
OLD SR-32
CLAIBORNE COUNTY
2025

FEDERAL PROJECT # NH/HSIP-32(104)			
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NO.	DATE	BY	BRIEF DESCRIPTION
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PLAN VIEW
13-SR32-4.41

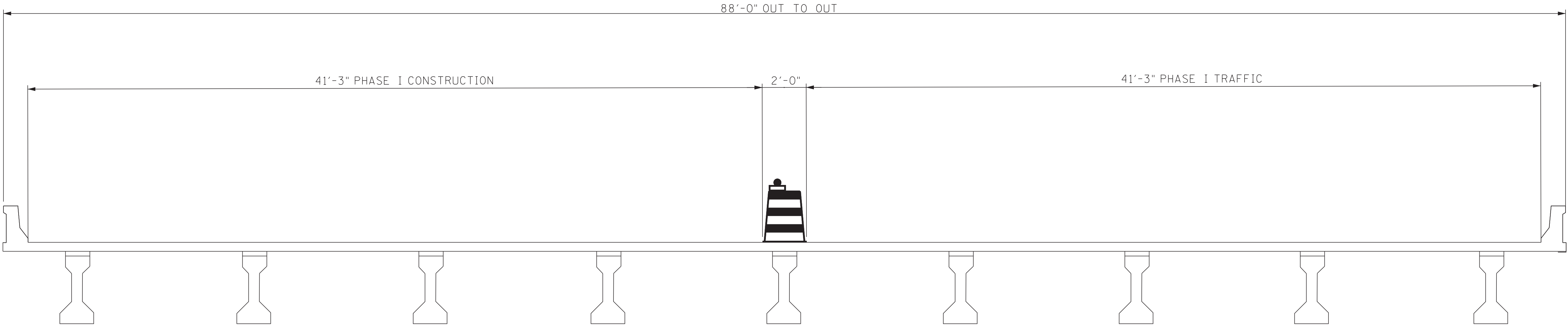
— DENOTES: APPROXIMATE JOINT REPAIRS LOCATIONS



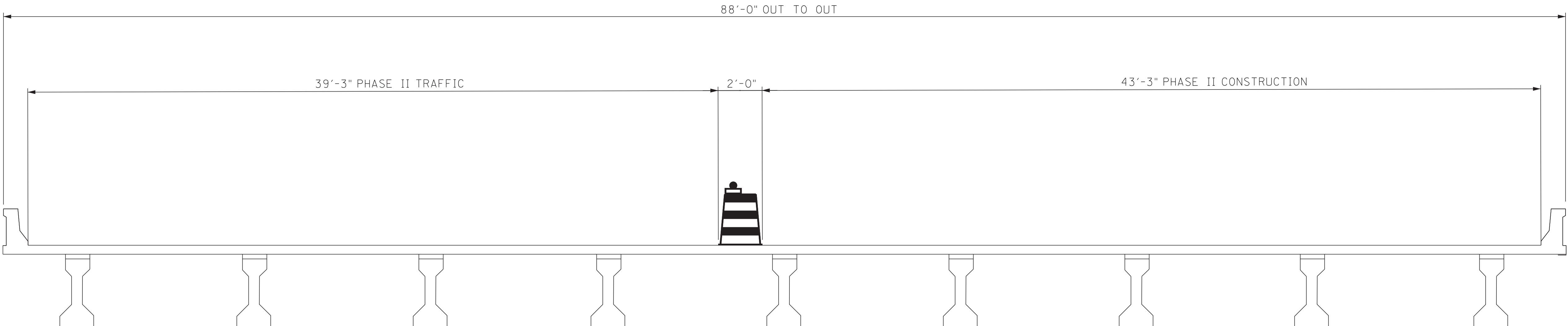
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PLAN VIEW
APPROXIMATE REPAIRS
LOCATIONS
13-SR32-4.41 OVER
LITTLE SYCAMORE CREEK
CLAIBORNE COUNTY
2025

PIN NO.:	134034.00	DATE:	/ /
DESIGN BY:	SILESHI ERGICHO	DATE:	2/25
DRAWN BY:	KEVIN MARTINKO	DATE:	2/25
SUPERVISED BY:		DATE:	/ /
CHECKED BY:		DATE:	/ /

FEDERAL PROJECT # NH/HSIP-32(104)			
PROJECT NO.		YEAR	SHEET NO.
13S032-M3-009		2025	B-4
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
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PHASE I CONSTRUCTION
(LOOKING AHEAD ON THE SURVEY)



PHASE II CONSTRUCTION
(LOOKING AHEAD ON THE SURVEY)

SEALED BY

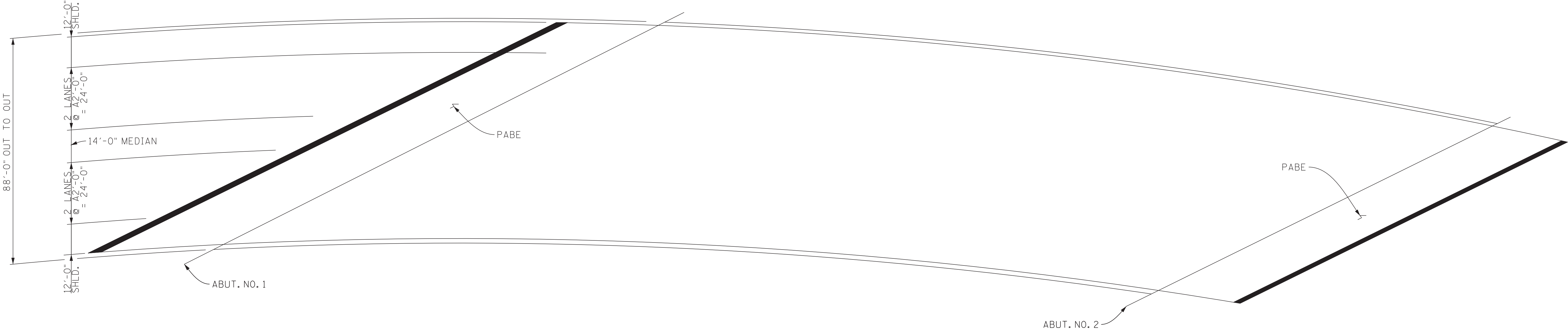
BRIAN K. EGLI
REGISTERED ENGINEER
No. 107196
STATE OF TENNESSEE

03/31/25

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PHASE CONSTRUCTION
13-SR32-4.41 OVER
LITTLE SYCAMORE CREEK
CLAIBORNE COUNTY
2025

PIN NO.: 134034.00
DESIGN BY: SILESHI ERGICHO
DRAWN BY: KEVIN MARTINKO
SUPERVISED BY: KEVIN MARTINKO
CHECKED BY:
DATE: 2/25
DATE: 2/25
DATE: / /

PROJECT NO.		YEAR	SHEET NO.
13S032-M3-009		2025	B-5
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PLAN VIEW
13-SR32-6.61

— DENOTES: APPROXIMATE JOINT REPAIRS LOCATIONS

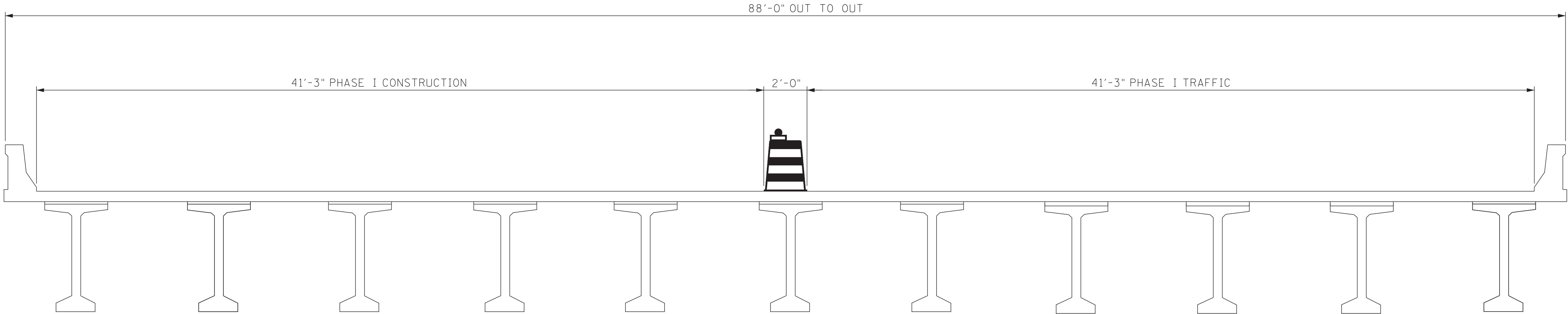
SEALED BY

BRIAN K. EGLI
REGISTERED ENGINEER
CIVIL ENGINEERING
STATE OF TENNESSEE
No. 107196
03/31/25

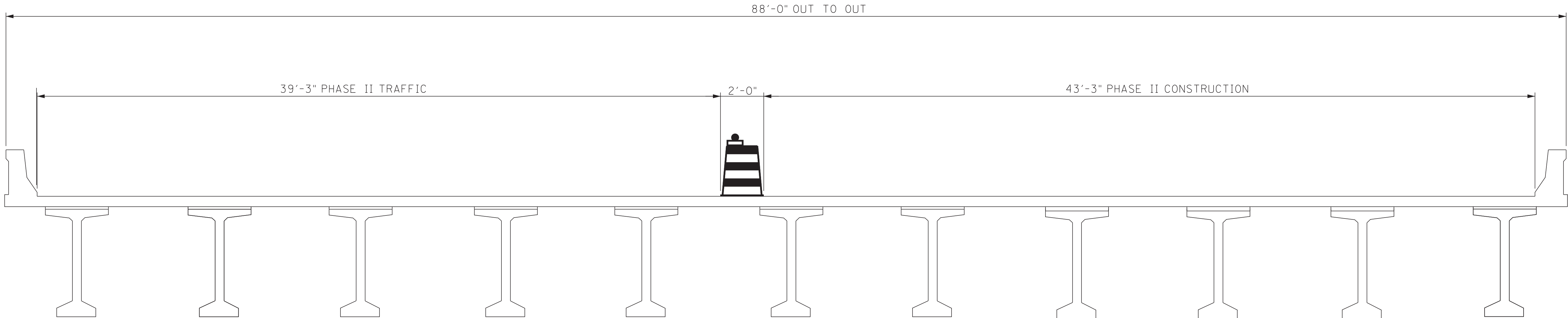
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PLAN VIEW
APPROXIMATE REPAIRS
LOCATIONS
13-SR32-6.61 OVER
OLD SR-32
CLAIBORNE COUNTY
2025

PIN NO.:	134034.00	DATE:	/ /
DESIGN BY:	SILESHEI ERGICHO	DATE:	2/25
DRAWN BY:	KEVIN MARTINKO	DATE:	2/25
SUPERVISED BY:		DATE:	/ /
CHECKED BY:		DATE:	/ /

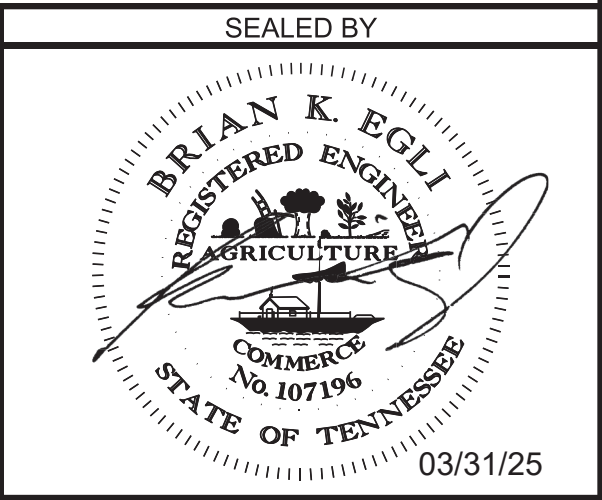
FEDERAL PROJECT # NH/HSIP-32(104)			
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PHASE I CONSTRUCTION
(LOOKING AHEAD ON THE SURVEY)



PHASE II CONSTRUCTION
(LOOKING AHEAD ON THE SURVEY)



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PHASE CONSTRUCTION
13-SR32-6.61 OVER
OLD SR-32
CLAIBORNE COUNTY
2025

PIN NO.:	134034.00	DATE:	/ /
DESIGN BY:		DATE:	2/25
DRAWN BY:	SILESHI ERGICHO	DATE:	2/25
SUPERVISED BY:	KEVIN MARTINKO	DATE:	2/25
CHECKED BY:		DATE:	/ /

\$\$\$\$SYTIME\$\$\$\$\$\$\$\$
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TYPE 1 THIN EPOXY OVERLAY NOTES :

TYPE 1 THIN EPOXY OVERLAY SYSTEM - USE DECK PRETREATMENT/PRIMER PER MANUFACTURER'S RECOMMENDATION, AND 2 LIFTS OF AN EPOXY-URETHANE COPOLYMER AND AGGREGATE. TYPE 1 OVERLAY SHALL BE APPLIED MECHANICALLY USING METERED EQUIPMENT; HAND MIXING OF MATERIAL IS NOT PERMITTED.

THIN OVERLAY SYSTEM SHALL BE FROM THE QUALIFIED PRODUCTS LIST 23.005 TYPE 1 THIN OVERLAY (EPOXY URETHANE). MINIMUM OVERLAY THICKNESS SHALL BE 3/8 INCH.

APPLICATION EQUIPMENT SHOULD :

A) BE CAPABLE OF METERING, MIXING AND DISTRIBUTING THE POLYMER AND PRETREATMENT TO MANUFACTURER'S RECOMMENDATION.

B) USE AN APPLICATION MACHINE THAT FEATURES POSITIVE DISPLACEMENT VOLUMETRIC METERING PUMPS CONTROLLED BY A HYDRAULIC POWER UNIT.

C) STORE COMPONENTS IN TEMPERATURE CONTROLLED RESERVOIRS CAPABLE OF MAINTAINING 100 DEGREES FAHRENHEIT (PLUS OR MINUS 10 DEGREES) TO INSURE OPTIMAL MIXING.

D) CHECK MIXING RATIO AT THE PUMP OUTLETS AS WELL AS CYCLE COUNTING CAPABILITIES TO MONITOR OUTPUT ON STANDARD FEATURES.

E) USE MOTIONLESS IN-LINE MIXING SO AS TO NOT OVERLY SHEAR THE MATERIAL TO ENTRAP AIR IN THE MIX.

F) MAXIMIZE MATERIAL WORKING TIME BY MIXING IT IMMEDIATELY BEFORE DISPENSING.

AGGREGATE SHALL BE ANGULAR, HAVING LESS THAN 0.2% MOISTURE AND FREE OF DIRT, CLAY, ASPHALT AND OTHER FOREIGN OR ORGANIC MATERIALS. AGGREGATE FOR ALL LAYERS SHALL BE BAUXITE OR FLINT ROCK PRODUCTS FLINT AND MEETS THE FOLLOWING GRADATION:

SIEVE SIZE	% PASSING
NO. 6	95-100
NO. 10	10-35
NO. 20	0-3

FULL AND PARTIAL DEPTH DECK REPAIR SHALL CURE A MINIMUM OF 28 DAYS BEFORE THE OVERLAY IS PLACED. THE 28 DAYS MAY BE WAIVED IF THE OVERLAY MANUFACTURER PROVIDES A METHOD OF TESTING THE REPAIRED AREAS AND APPROVES THE PLACEMENT BY LETTER. TRAFFIC SHALL BE ALLOWED TO USE THE BRIDGE DURING THE CURING PERIOD OF THE PATCHES BUT NOT AFTER SHOTBLASTING. MAGNESIUM PHOSPHATE BASED MATERIALS WILL NOT BE ALLOWED.

THE CONCRETE DECK SURFACE SHALL BE CLEANED BY SHOTBLASTING TO REMOVE ANY OIL, DIRT, RUBBER, TRAFFIC STRIPING, OR ANY OTHER POTENTIAL DETRIMENTAL MATERIAL SUCH AS CURING COMPOUND AND LAITANCES, WHICH THE MANUFACTURER AND ENGINEER'S OPINION WOULD PREVENT PROPER BONDING AND CURING OF THE MATERIAL. IN AREAS WHERE SHOTBLASTING EQUIPMENT CAN NOT REACH (I.E., ALONG CURBS AND BRIDGE RAILS) SANDBLASTING IS PERMITTED TO AN EXTENT TO THE ENGINEER'S AND MANUFACTURER'S APPROVAL. IMMEDIATELY BEFORE APPLICATION, ALL PREPARED SURFACES SHALL BE CLEANED WITH COMPRESSED AIR OR VACUUMED TO REMOVE DUST AND DEBRIS. THE CONTRACTOR IS TO PREVENT THE TRACKING OF TACK COAT AND CONSTRUCTION DEBRIS ACROSS THE BRIDGE DECK PRIOR TO THE APPLICATION OF THE THIN OVERLAY. MILLING THE BRIDGE DECK WILL NOT BE AN OPTION FOR TACK COAT OR DEBRIS REMOVAL. REMOVAL SHALL BE AT THE CONTRACTOR'S EXPENSE.

ALL SURFACES THAT ARE TREATED SHALL BE DRY AT THE TIME OF APPLICATION. THE OVERLAY SHALL NOT BE APPLIED WHEN IT HAS RAINED 24 HOURS PRIOR TO, OR RAIN IS FORECAST WITHIN 8 HOURS AFTER, APPLICATION. THE MOISTURE CONTENT IN THE DECK SUBSTRATE SHALL BE TESTED. MOISTURE IS NOT TO EXCEED 4.5 PERCENT WHEN MEASURED BY ELECTRONIC METER. IF THE TEST SHOWS EXCESS MOISTURE, THE DECK SHALL CONTINUE TO DRY BEFORE APPLICATION PROCEEDS.

BLUSHING (A WAXY SURFACE COATING ON THE EPOXY) IS CAUSED BY THE REACTION OF MOISTURE WITH THE HARDENING AGENT. BLUSHING CREATES A SURFACE THAT MAKES FUTURE LAYERS DIFFICULT TO ADHERE. LIFTS THAT SHOW SIGNS OF BLUSHING SHALL BE REMOVED AND REPLACED PRIOR TO APPLICATION OF THE NEXT. THE COST TO REMOVE AND REPLACE THESE AREAS SHALL BE AT THE CONTRACTOR'S EXPENSE.

TRAFFIC, OTHER THAN APPLICATION EQUIPMENT, SHALL NOT BE ALLOWED ON ANY PORTION OF THE DECK THAT HAS BEEN SHOTBLASTED OR WHERE PART OF THE APPLICATION HAS BEEN PLACED.

SEE MANUFACTURER'S RECOMMENDATIONS FOR REQUIRED AMBIENT AND SURFACE TEMPERATURES AND HUMIDITY LIMITS FOR APPLICATION.

THE MANUFACTURER SHALL HAVE A REPRESENTATIVE ON THE JOB SITE AT ALL TIMES DURING APPLICATION AND CURE TIME. THE REPRESENTATIVE, ALONG WITH CONSULTATION WITH ENGINEER, MAY SUSPEND ANY ITEM OF WORK THAT IS SUSPECT AND DOES NOT MEET THE REQUIREMENTS OF THE SPECIFICATIONS. WORK SHALL NOT RESUME UNTIL THE ENGINEER AND REPRESENTATIVE ARE SATISFIED THAT APPROPRIATE REMEDIAL ACTION HAS BEEN TAKEN BY THE CONTRACTOR.

ALL COSTS FOR AGGREGATE, EPOXY FOR MINIMUM OF TWO LIFTS, SURFACE PREPARATION, LABOR AND ANY OTHER MISCELLANEOUS MATERIALS REQUIRED TO PLACE THIN OVERLAY SHALL BE INCLUDED IN ITEM NO. 617-04.01, TYPE 1 THIN EPOXY OVERLAY (EPOXY URETHANE), S.Y.

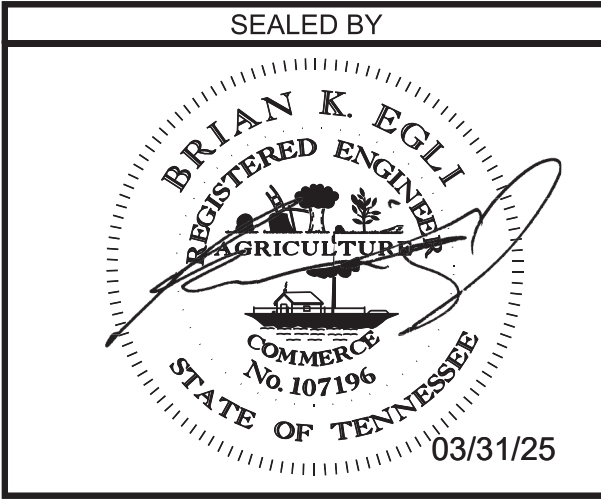
THICKNESS VERIFICATION: THE PROJECT ENGINEER SHALL BE NOTIFIED OF THE NUMBER OF GALLONS USED ON THE PROJECT WITH NOTARIZED QUANTITY STATEMENTS FROM THE CONTRACTOR AND THE MANUFACTURER. THE CONTRACTOR SHALL VERIFY TO TDOT THAT THE OVERLAY IS AN AVERAGE OF AT LEAST 3/8 INCH THICK AT THREE RANDOM LOCATIONS AGREED UPON BY THE PROJECT ENGINEER AND THE MATERIAL MANUFACTURER REPRESENTATIVE. IF 3/8 INCH AVERAGE IS NOT ACHIEVED, A RETEST SHALL BE PERFORMED IN ADJOINING AREAS. THIN AREAS SHALL BE RE-COATED AS DESCRIBED ABOVE BY THE CONTRACTOR AND RE-VERIFIED AT NO ADDITIONAL COST TO TDOT. THIS VERIFICATION MAY CONSIST OF CORES MADE BY THE CONTRACTOR WITH A CORING BIT NOT LESS THAN 1½" DIAMETER, THE TESTED AREAS SHALL BE REPAIRED BY THE CONTRACTOR BEFORE FINAL ACCEPTANCE BY THE PROJECT ENGINEER.

THE MANUFACTURER SHALL HAVE A REP ON THE JOB SITE AT ALL TIME DURING APPLICATION AND CURE TIME. THE REP WITH THE ENGINEER, MAY SUSPEND AN ITEM OF WORK THAT IS SUSPECT AND DOES NOT MEET THE REQUIREMENTS OF THE SPECS. WORK SHALL NOT RESUME UNTIL THE ENGINEER AND REP ARE SATISFIED THAT APPROPRIATE REMEDIAL ACTION HAS BEEN TAKEN BY THE CONTRACTOR.

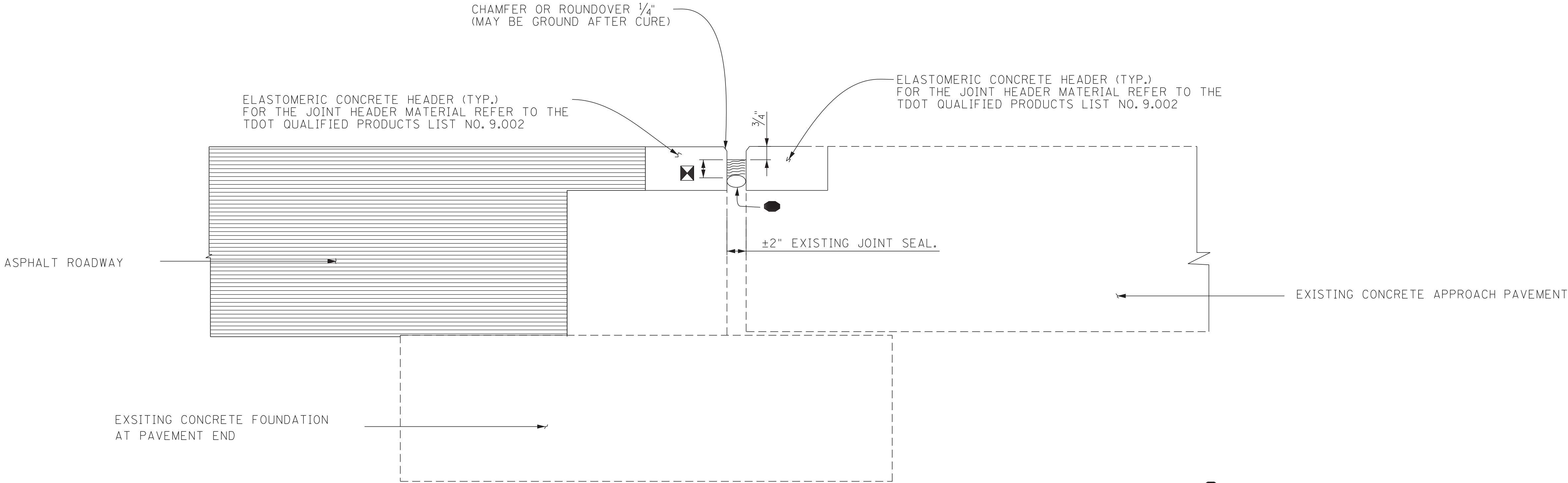
PIN NO.:	134034.00	DATE:	/ /
DESIGN BY:		DATE:	2/25
DRAWN BY:	SILESHI ERGICHO	DATE:	2/25
SUPERVISED BY:	KEVIN MARTINKO	DATE:	/ /
CHECKED BY:		DATE:	/ /

PROJECT NO.		YEAR	SHEET NO.
13S032-M3-009		2025	B-7
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION
	- -		
	- -		
	- -		
	- -		
	- -		

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
TYPE 1 THIN EPOXY
OVERLAY NOTES
13-SR32-4.41 OVER
LITTLE SYCAMORE CREEK
CLAIBORNE COUNTY
2025



PROJECT NO.		YEAR		SHEET NO.	
13S032-M3-009		2025		B-8	
REVISIONS					
NO.	DATE	BY	BRIEF DESCRIPTION		
	- -				
	- -				
	- -				
	- -				
	- -				



● NOTE: THE EXISTING JOINT OPENING SHALL BE CAULKED WITH A BACKER ROD OF SUITABLE DIAMETER,THE ROD SHALL BE PLACED AT A DEPTH TO INSURE THE CORRECT WIDTH/DEPTH RATIO OF THE NEW JOINT SEALANT MATERIAL,BACKER ROD SHALL BE AS PER JOINT MANUFACTURERS RECOMMENDATIONS.

☒ NOTE: FULL DEPTH OF ALL EXISTING JOINTS SHALL SHALL BE RESEALED WITH NEW JOINT SEALER. THE NEW JOINT SEALER SHALL BE A COLD PUOR TWO PART SILICONE SEALER FROM QPL 5.001.

EXPANSION JOINT SYSTEM

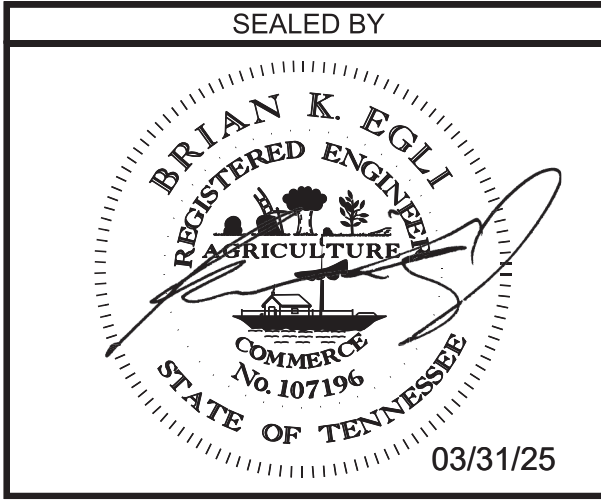
EXPANSION JOINT REPAIR NOTES:

THE JOINT HEADER SYSTEM SHALL BE FROM QPL 9.002. THE JOINT SEAL SYSTEM CONSISTS OF THE ELASTOMERIC HEADER MATERIAL AND A TWO-PART COLD POUR SILICONE SEALANT. THE CONTRACTOR SHALL ALSO HAVE THE OPTION OF USING A PRECOMPRESSED FOAM WITH SILICONE TOPPED PRODUCT (SIMILAR TO AND INCLUDING BEJS) FROM QPL 7.001 WITH AN APPROPRIATELY SIZED SEAL FOR THE JOINT OPENING,AND AN ELASTOMERIC CONCRETE FROM QPL 9.001.THE SYSTEM SHALL BE INSTALLED UNDER THE DIRECT SUPERVISION OF AN AUTHORIZED TECHNICIAN PROVIDED BY THE SYSTEM MANUFACTURER. THE TECHNICIAN MUST APPROVE ALL ASPECTS OF THE GEOMETRY AND PREPARATION OF THE JOINT LOCATIONS PRIOR TO ANY INSTALLATION OF THE JOINT SYSTEM MATERIALS. PRODUCTS FROM QPL 7.001 "ON A ROLL" AND "COMPRESSION ONLY" WILL NOT BE ALLOWED. THE TOP OF THE QPL 7.001 JOINT FILLER SHALL BE A MINIMUM OF 3/4 OF AN INCH BELOW THE ROADWAY SURFACE UNLESS THE MANUFACTURER REQUIRES A DEEPER DEPTH.

MANUFACTURERS SPECIFICATIONS AND INSTALLATION PROCEDURES SHALL BE SUBMITTED TO THE PROJECT ENGINEER FOR REVIEW PRIOR TO THE JOINT REPLACEMENT/REPAIR WORK. THE MANUFACTURER AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE WORKMANSHIP OF THE JOINT INSTALLATION.

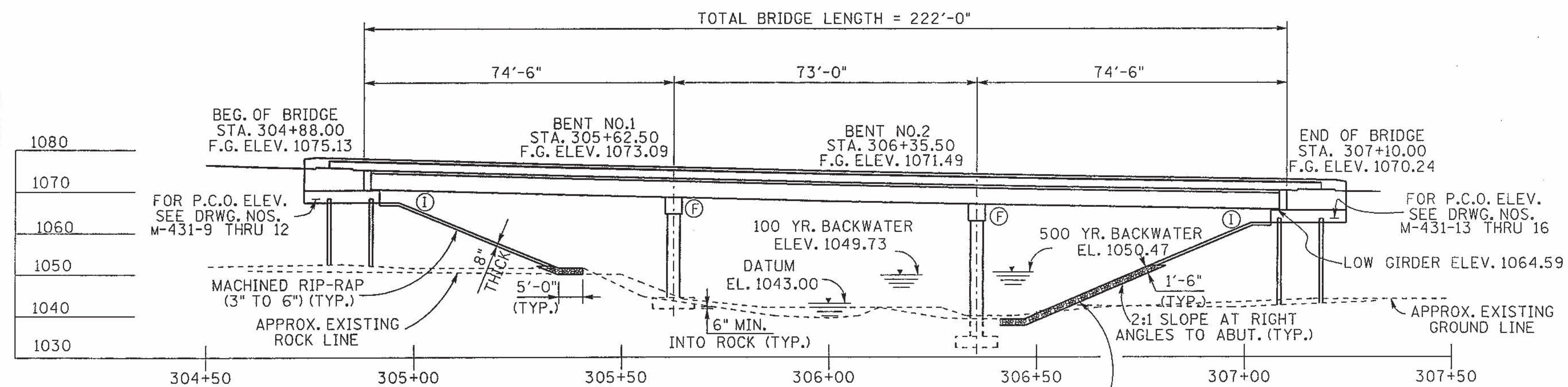
PRIOR TO THE INSTALLATION OF THE NEW JOINT,THE EXISTING JOINT OPENING SHALL BE CLEANED OF ALL DIRT,DEBRIS,AND PRIOR CONSTRUCTION MATERIAL,ETC.,THE FULL DEPTH OF THE OPENING. THE SURFACES WHERE THE NEW MATERIAL BONDS TO STAY IN PLACE,SHALL BE CLEANED PER MANUFACTURERS RECOMMENDATION,TO REMOVE ANY SUBSTANCES THAT WOULD INHIBIT BONDING.

THE COST FOR REMOVING THE OLD JOINT SYSTEM,INSTALLING THE NEW JOINT SYSTEM,LABOR,AND ANY MISCELLANEOUS MATERIALS NECESSARY TO INSTALL THE NEW EXPANSION JOINT,IS TO BE INCLUDED UNDER ITEM NUMBER 604-10.44,EXPANSION JOINT REPAIRS,L.F.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
EXPANSION JOINT
DETAILS AT APPROACH
PAVEMENTS ENDS
13-SR32-4.41 OVER
LITTLE SYCAMORE CREEK
AND 13-SR32-6.61 OVER
OLD SR-32
CLAIBORNE COUNTY
2025

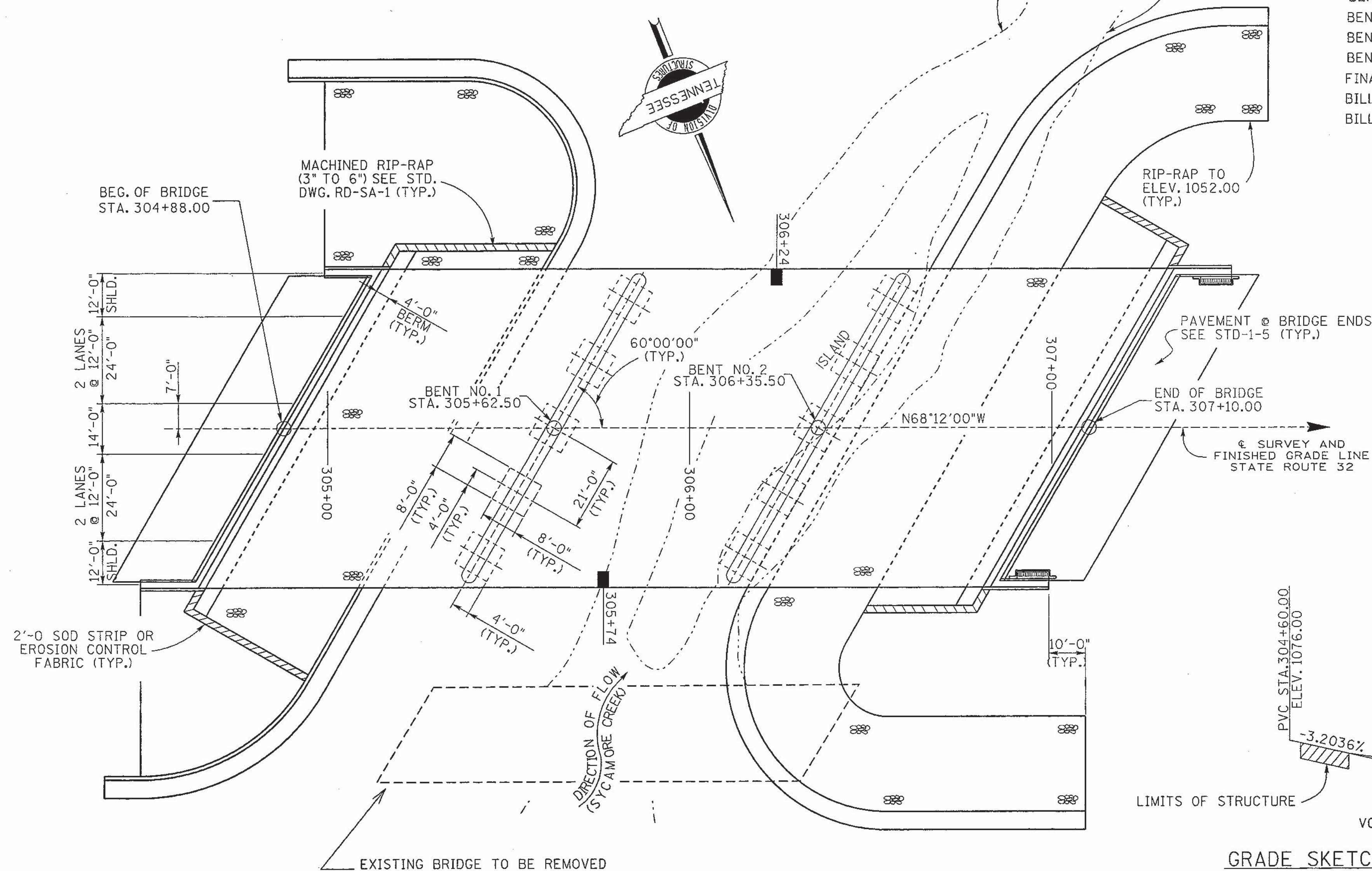
PIN NO.:	134034.00	DATE:	/ /
DESIGN BY:		DATE:	2/25
DRAWN BY:	SILESHI ERGICHO	DATE:	2/25
SUPERVISED BY:	KEVIN MARTINKO	DATE:	/ /
CHECKED BY:		DATE:	/ /



(F) DENOTES: FIXED
(I) DENOTES: INTEGRAL

NOTE: MAINTAIN TRAFFIC ON EXISTING
STRUCTURE DURING CONSTRUCTION.

ELEVATION



PLAN

■ DENOTES: END OF BRIDGE DRAINS.
SEE STD. DWGS. NO. STD-1-6,7 AND 9.

■ DENOTES: BRIDGE DECK DRAIN REQUIRED.
SEE STD. DWG. NO. STD-1-2.
USE GRATE DRAIN TYPE 2.

HYDRAULIC DATA

DRAINAGE AREA = 13.8 SQ. MILES
DESIGN DISCHARGE (100 YR.) = 3,120 CFS.
WATER AREA PROVIDED BELOW EL. 1050.04 = 953 SQ. FT.
100 YEAR VELOCITY = 3.28 FT./SEC.
100 YR. BRIDGE BACKWATER = 0 FT. @ EL. 1049.73
ROADWAY OVERTOPPING EL. 1066.63
500 YEAR DISCHARGE = 4,140 CFS AT ELEV. 1050.47

LIST OF DRAWINGS

	DWG. NO.	LAST REV. DATE
LAYOUT OF BRIDGE	M-431-1	
GENERAL NOTES AND ESTIMATED QUANTITIES	M-431-2	
FOUNDATION DATA	M-431-3	
SUPERSTRUCTURE	M-431-4	
SUPERSTRUCTURE DETAILS	M-431-5	
SUPERSTRUCTURE DETAILS	M-431-6	
SUPERSTRUCTURE DETAILS	M-431-7	
PRESTRESSED I BEAM DETAILS (SPAN 1,2&3)	M-431-8	
ABUTMENT NO. 1	M-431-9	
ABUTMENT NO. 1 DETAILS	M-431-10	
ABUTMENT NO. 1 DETAILS	M-431-11	
ABUTMENT NO. 1 DETAILS	M-431-12	
ABUTMENT NO. 2	M-431-13	
ABUTMENT NO. 2 DETAILS	M-431-14	
ABUTMENT NO. 2 DETAILS	M-431-15	
ABUTMENT NO. 2 DETAILS	M-431-16	
BENT NO. 1	M-431-17	
BENT NO. 1 DETAILS	M-431-17A	
BENT NO. 1 DETAILS	M-431-17B	
BENT NO. 2	M-431-18	
BENT NO. 2 DETAILS	M-431-18A	
BENT NO. 2 DETAILS	M-431-19	
FINAL FOUNDATION DATA	M-431-20	
BILL OF STEEL	M-431-21	
BILL OF STEEL	M-431-22	

LIST OF STANDARD DRAWINGS DWG. NO. REV. DATE

BRIDGE RAILING CONCRETE PARAPET	STD-1-1	07-31-00
SLIDER PLATES AND DECK DRAINS	STD-1-2	01-05-01
REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS	STD-1-5	07-31-00
BRIDGE END DRAIN WITH PAVEMENT AT BRIDGE ENDS	STD-1-6	04-28-97
BRIDGE END DRAIN WITH PAVEMENT AT BRIDGE ENDS	STD-1-7	07-31-00
BRIDGE END DRAIN 4' X 8'-7" WITH PAVEMENT AT BRIDGE ENDS	STD-1-9	05-01-95
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STD-4-1	05-21-99
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS DESIGN CRITERIA	STD-4-2	06-10-96
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STD-4-3	06-10-96
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS CONSTRUCTION DETAILS	STD-4-4	06-10-96
STANDARD PILE DETAILS	STD-5-1	10-25-93
STANDARD PILE DETAILS	STD-5-2	05-21-99
STANDARD SEISMIC DETAILS	STD-6-1	05-21-99
STANDARD SEISMIC DETAILS	STD-6-2	11-07-94
REINF. BAR SUPPORT DETAILS FOR CONC. SLABS	STD-9-1	12-19-94
MISCELLANEOUS ABUTMENT & DRAINAGE DETAILS	STD-10-1	05-11-92
STD. DETAILS AND INT. DIAPH.		
DETAILS FOR I BEAMS	STD-14-2	07-31-00
SLOPE PROTECTION	RD-SA-1	05-27-98

LIST OF SPECIAL PROVISIONS PROV. NO. REV. DATE

APPROVAL OF SHOP DRAWINGS	105A	12-15-97
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2016 ADT= 14480
1 @ 86'-0" ROADWAY
W/STD-1-1 PARAPET
DESIGN SPEED = 60 MPH.

STATE OF TENNESSEE

DEPARTMENT OF TRANSPORTATION

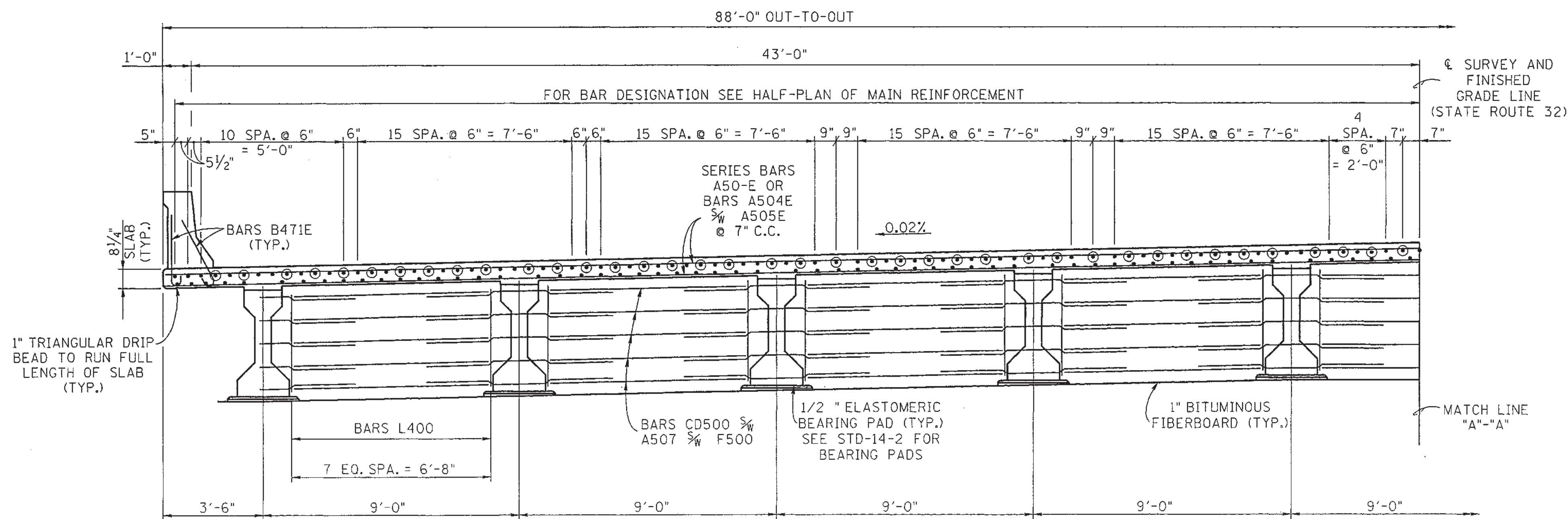
BRIDGE NO. 1
LAYOUT OF BRIDGE
STATE ROUTE 32

OVER
LITTLE SYCAMORE CREEK
BRIDGE I.D. NO. 13SR0320007
STATION 306+00.00
LOG MILE 4.45
CLAIBORNE COUNTY
2002



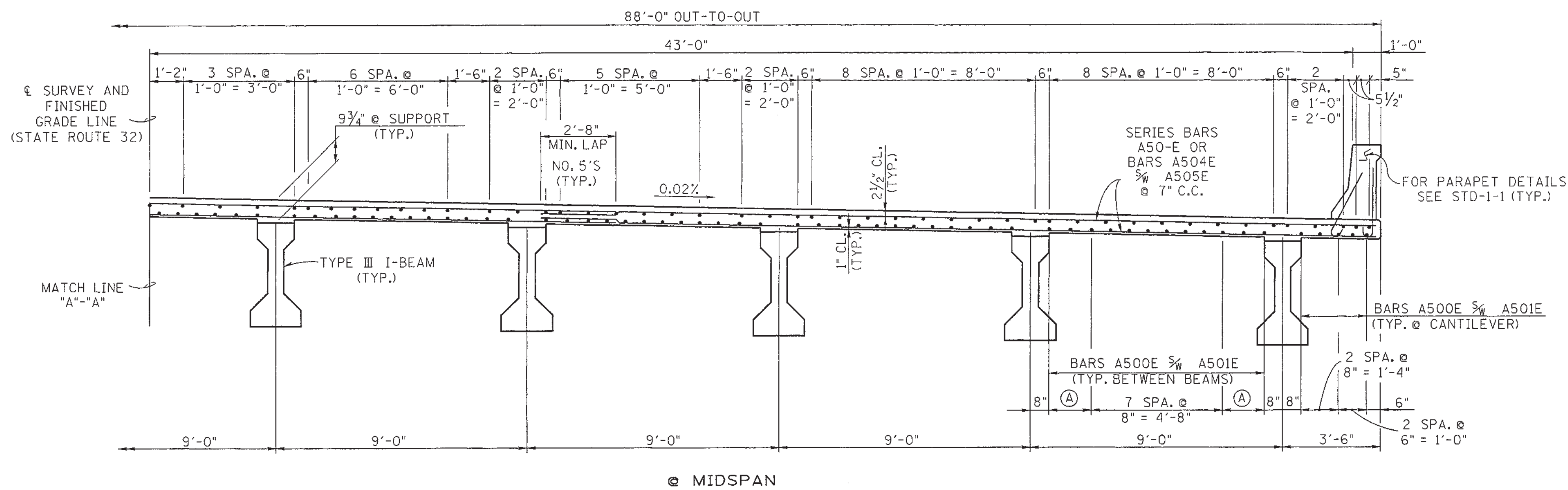
CORRECT *Edward P. Wasserman*
ENGINEER OF STRUCTURES

DESIGNED BY D. HOWARD DATE 5-96
DRAWN BY C. STAPLER DATE 4-02
SUPERVISED BY J.W.F. / C.M.D. DATE 4-02
CHECKED BY C. DIETERS DATE 4-02



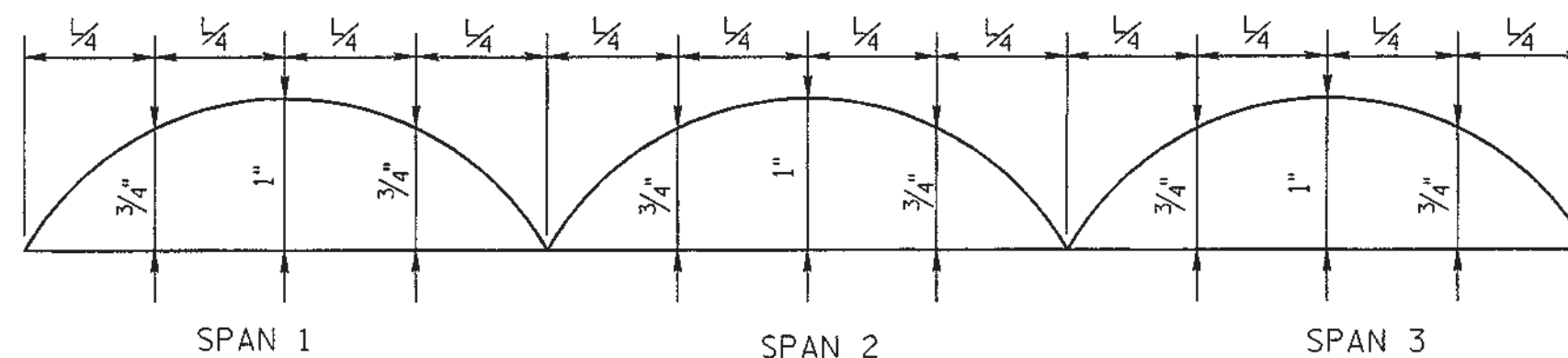
⊙ DENOTES: BAR CUTOFFS.

@ SUPPORT



TYPICAL CROSS SECTION
(LOOKING FORWARD ON SURVEY)

(A) DENOTES: 2 SPA. @ 9" = 1'-6"



L DENOTES: SPAN LENGTH

DEAD LOAD CORRECTION CURVE

DEAD LOAD CORRECTION CURVE: THIS CURVE IS FOR DEAD LOAD SLAB AND ALL DEAD LOADS THAT ARE APPLIED AFTER SLAB IS IN PLACE AND SHOULD BE CORRECTED TO COMPENSATE FOR THE EFFECTS DUE TO VERTICAL CURVE. IF PRESTRESSED DECK PANELS ARE USED AND THE BEAMS ARE PROFILED AFTER PANELS ARE IN PLACE, REDUCE THE DEAD LOAD CORRECTION VALUES SHOWN BY 25%.

GENERAL NOTES:

NOTE: NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.

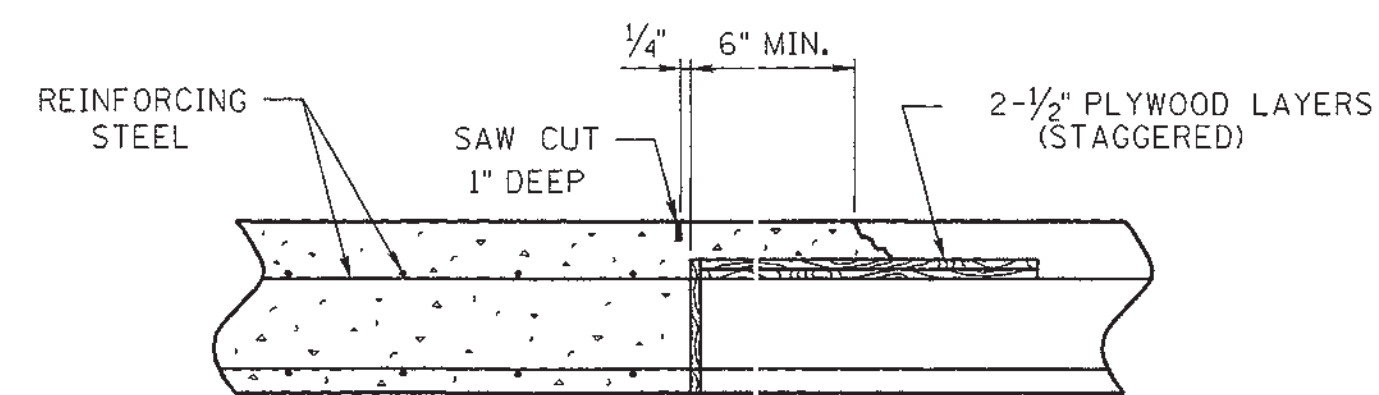
NOTE: THE CONTRACTOR IS SOLELY RESPONSIBLE FOR SUPPORTING THE BEAMS TO PREVENT DAMAGE DUE TO TWISTING OR OVERTURNING DURING ALL PHASES OF CONSTRUCTION. IT IS STRONGLY RECOMMENDED THAT THE TEMPORARY ERECTION DIAPHRAGMS BE PRIOR TO PLACING ANY LOADS ON THE GIRDERS HOWEVER, TEMPORARY ERECTION DIAPHRAGMS MUST BE IN PLACE IN THE SPAN AT THE TIME THE SLAB IS POURED IN SAID SPAN.

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED.

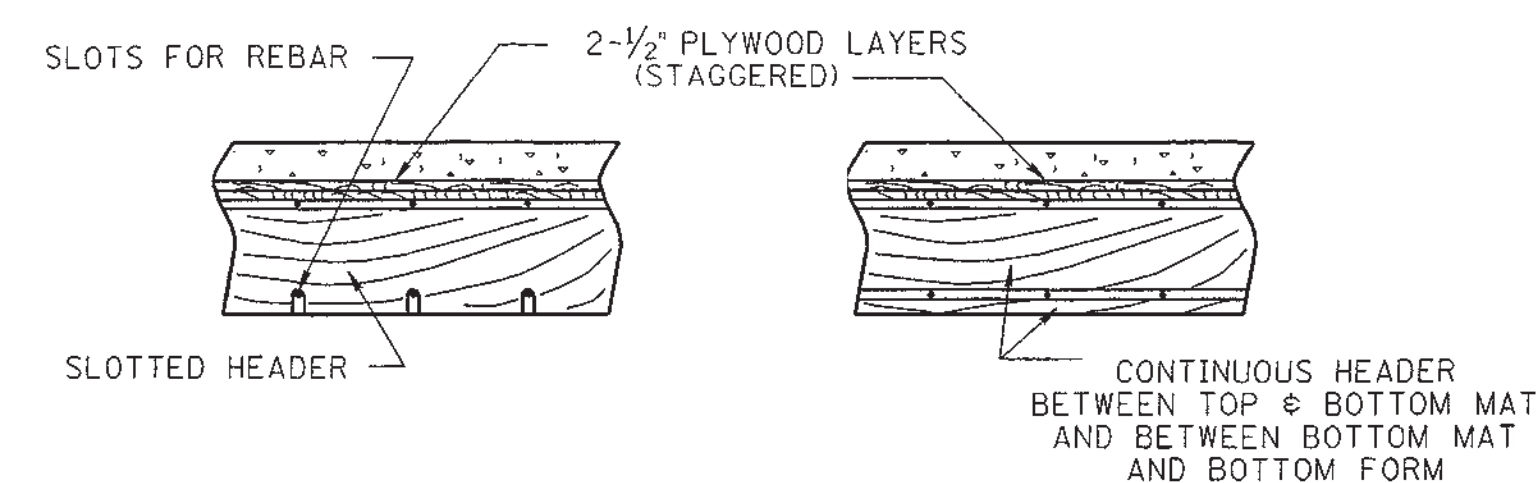
ALSO SEE STANDARD DWG. NO. STD-1-1.

DECK CONCRETE POURING SEQUENCE: SLAB CONSTRUCTION JOINTS MAY BE LOCATED AT THE CONTRACTOR'S OPTION SUBJECT TO THE FOLLOWING:

1. NO CONSTRUCTION JOINT MAY BE LOCATED CLOSER THAN 10 FEET OR FURTHER THAN 15 FEET FROM AN INTERIOR SUPPORT.
2. THE SLAB IN THE MIDDLE SECTION OF BOTH ADJACENT SPANS MUST BE POURED TO WITHIN AT LEAST 15 FEET OF THE SUPPORTS EITHER PRIOR TO OR CONCURRENTLY WITH THE SLAB OVER AN INTERIOR SUPPORT.
3. ALL SLAB CONSTRUCTION JOINTS SHALL BE IN ACCORDANCE WITH THE SLAB CONSTRUCTION JOINT DETAIL SHOWN BELOW.



SLAB CONSTRUCTION JOINT DETAIL



ALTERNATE "A"

ALTERNATE "B"

ALTERNATE HEADER DETAILS

ESTIMATED QUANTITIES		
CONCRETE CLASS "D" (BRIDGE DECK) C.Y.	EPOXY COATED REINFORCING STEEL LB.	REINFORC- ING STEEL L.B.
550	168,273	3,958

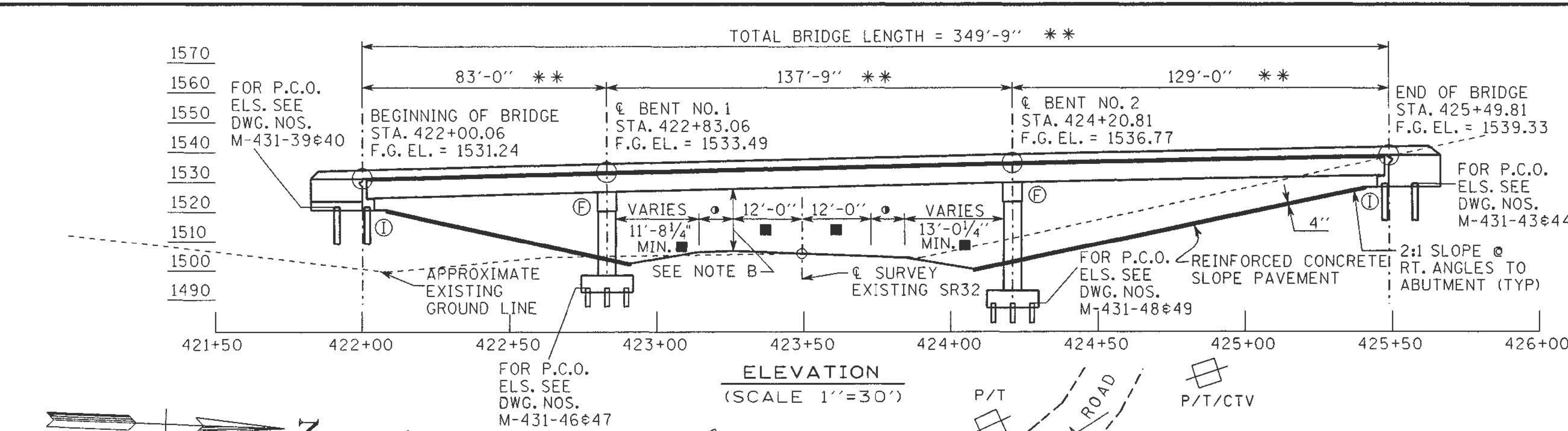


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BRIDGE NO. 1
SUPERSTRUCTURE
STATE ROUTE 32
OVER
LITTLE SYCAMORE CREEK
STATION 306+00.00
CLAIBORNE COUNTY
2002

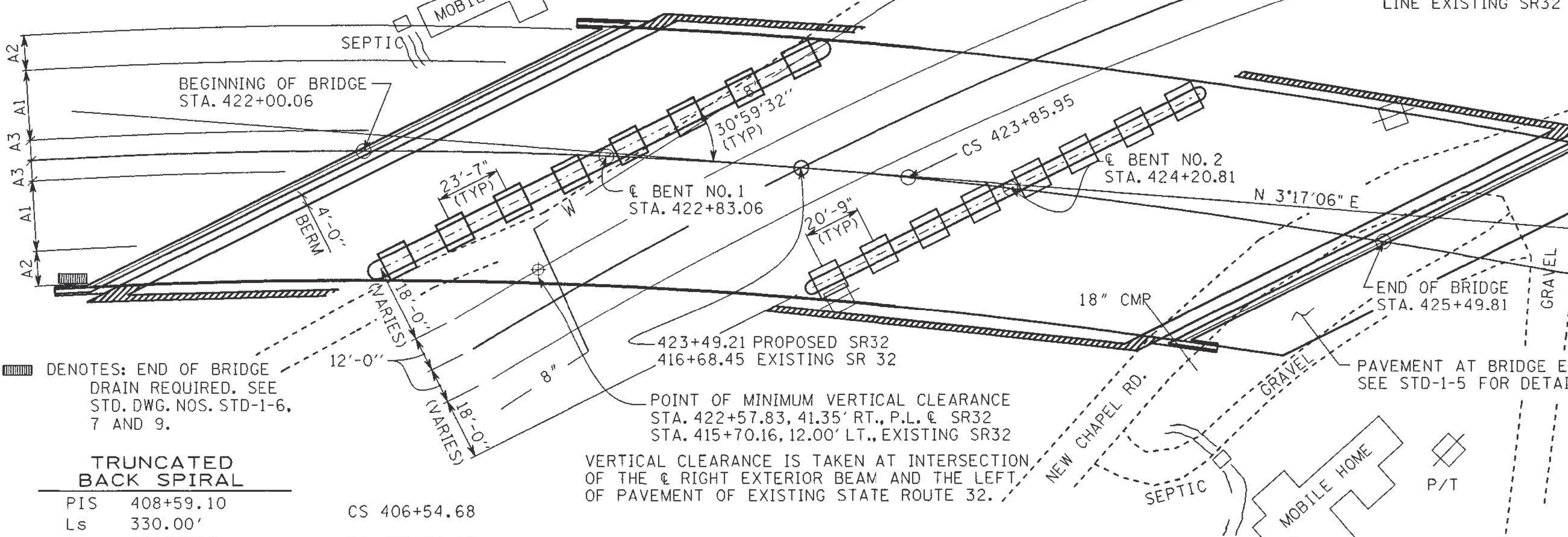
CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

M-431-4

DESIGNED BY D. HOWARD DATE 04-02
DRAWN BY G. W. KORNIKOSKI DATE 04-02
SUPERVISED BY C.M.D./J.W.F. DATE 04-02
CHECKED BY C. DIETERS DATE 04-02



A1 DENOTES: 2 LANES @ 12'-0" = 24'-0"
A2 DENOTES: 12'-0" SHOULDER
A3 DENOTES: 7'-0" MEDIAN



TRUNCATED BACK SPIRAL	
PIS	408.59.10
Ls	330.00'
Δs	5°46'30"
Rc1	11459.16'
Rc2	1909.86'
LT	204.42'
ST	125.83'
LC	329.86'

CS 406+54.68
SC 409+84.68
CS 423+85.95
ST 427+45.95

SIMPLE CURVE	
PI	417+18.54
N	771391.09
E	2687068.51
Δ	42°02'18" RT
Dc	3°00'00"
T	733.86'
L	1401.27'
E	136.14'
R	1909.86'
SE	0.068 Fk

EXISTING SR 32	
CURVE DATA	
CURVE NO. 6	
PI	417+17.54
N	772059.75
E	2687005.74
Δ	41°34'00" RT
Dc	03°50'48"
T	565.31'
L	1080.59'
E	103.67'
R	1489.49'
L.C.	1057.05'
M.O.	96.92'

VERTICAL CLEARANCE IS TAKEN AT INTERSECTION
OF THE C RIGHT EXTERIOR BEAM AND THE LEFT
OF PAVEMENT OF EXISTING STATE ROUTE 32.

NEW

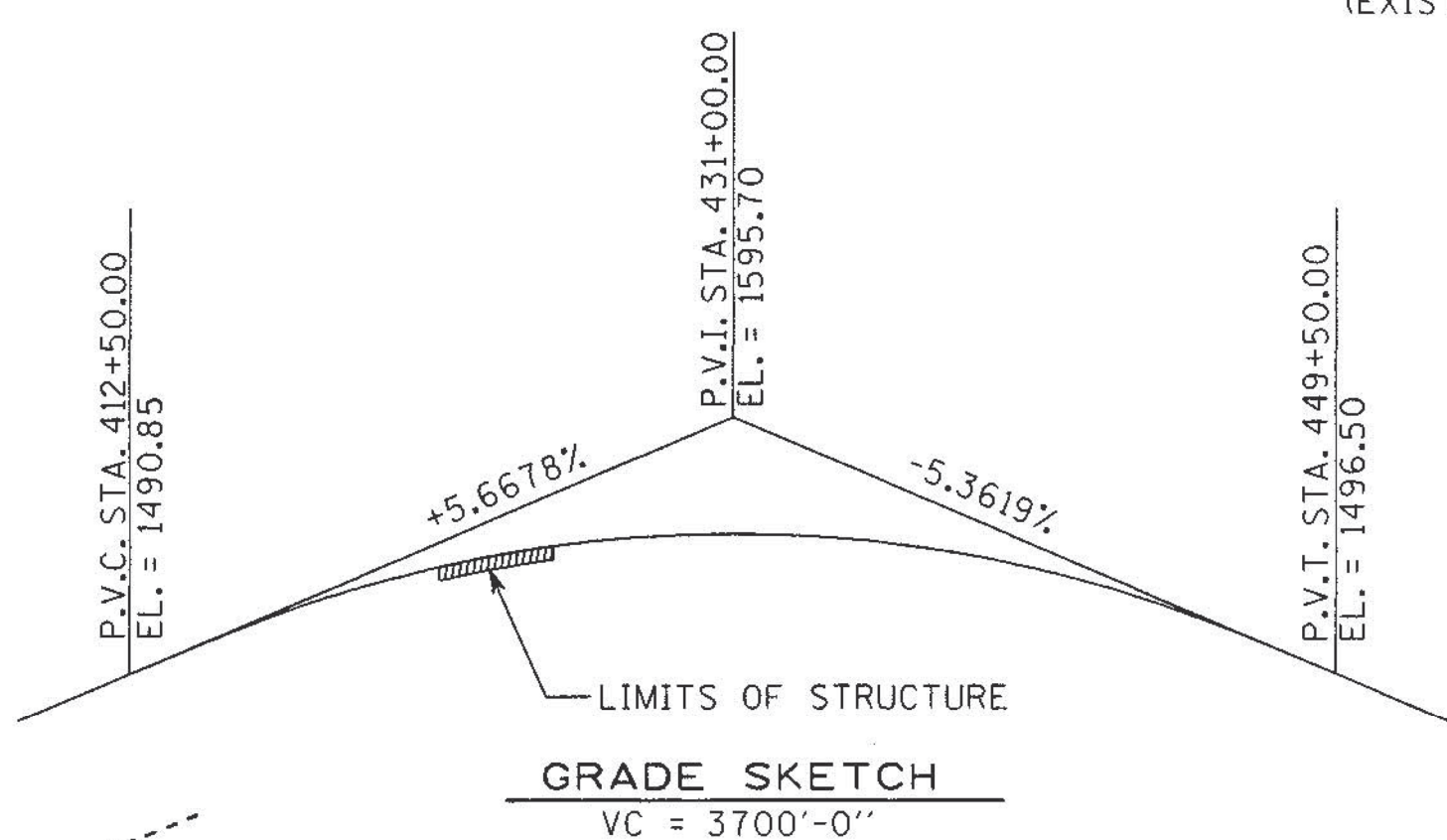
PLAN
(SCALE 1"=30')

NOTE: WHERE UNDERGROUND PIPE LINES MAY BE
ENCOUNTERED DURING CONSTRUCTION, IT SHALL
BE THE CONTRACTORS RESPONSIBILITY TO
PROTECT THESE LINES FROM ANY DAMAGE.
WHERE RELOCATION OR READJUSTMENT OF
A UTILITY MAY BE REQUIRED, IT SHALL BE
DONE BY THE OWNER OF THE SAME.

AHEAD SPIRAL	
PIS	425+06.06
Ls	360.00'
Øs	5'24'00"
Xs	359.68'
Ys	11.30'
LT	240.11'
ST	120.10'
LC	359.86'

<u>LIST OF SPECIAL PROVISIONS</u>	<u>PROV. NO.</u>	<u>REV. DATE</u>
APPROVAL OF SHOP DRAWINGS	105A	12-15-97

* * DENOTES: MEASURED ALONG € SURVEY PROPOSED SR32
 ① DENOTES: INTEGRAL
 ② DENOTES: FIXED
 ● DENOTES: 6'-0" ■
 NOTE B: REQUIRED CLEARANCE = 16'-6"
 ACTUAL CLEARANCE = 18'-9 7/8"
 ■ DENOTES: MEASURED PERPENDICULAR
 TO EXISTING STATE ROUTE 32



<u>LIST OF DRAWINGS</u>	<u>DWG. NO.</u>	<u>REV. DATE</u>
LAYOUT OF BRIDGE	M-431-23	-----
GENERAL NOTES & ESTIMATED QUANTITIES	M-431-24	-----
FOUNDATION DATA	M-431-25	-----
FOUNDATION DATA	M-431-25A	-----
SUPERSTRUCTURE	M-431-26	-----
SUPERSTRUCTURE DETAILS	M-431-27	-----
SUPERSTRUCTURE DETAILS	M-431-28	-----
SUPERSTRUCTURE DETAILS	M-431-29	-----
SUPERSTRUCTURE DETAILS	M-431-30	-----
SUPERSTRUCTURE DETAILS	M-431-31	-----
SUPERSTRUCTURE DETAILS	M-431-32	-----
SUPERSTRUCTURE DETAILS	M-431-33	-----
PRESTRESSED BULB TEE BEAM DETAILS (SPAN 1, BEAM NOS. 1-11)	M-431-34	-----
PRESTRESSED BULB TEE BEAM DETAILS (SPAN 2, BEAM NOS. 12&13)	M-431-35	-----
PRESTRESSED BULB TEE BEAM DETAILS (SPAN 2, BEAM NOS. 14-16)	M-431-36	-----
PRESTRESSED BULB TEE BEAM DETAILS (SPAN 2, BEAM NOS. 17-21)	M-431-37	-----
PRESTRESSED BULB TEE BEAM DETAILS (SPAN 3, BEAM NOS. 22-33)	M-431-38	-----
ABUTMENT NO. 1	M-431-39	-----
ABUTMENT NO. 1	M-431-40	-----
ABUTMENT NO. 1	M-431-41	-----
ABUTMENT NO. 1 DETAILS	M-431-42	-----
ABUTMENT NO. 2	M-431-43	-----
ABUTMENT NO. 2	M-431-44	-----
ABUTMENT NO. 2 DETAILS	M-431-45	-----
BENT NO. 1	M-431-46	-----
BENT NO. 1	M-431-47	-----
BENT NO. 2	M-431-48	-----
BENT NO. 2	M-431-49	-----
BENT NOS. 1 & 2 DETAILS	M-431-50	-----
FINAL FOUNDATION DATA	M-431-51	-----
FINAL FOUNDATION DATA	M-431-52	-----
BILL OF STEEL	M-431-53	-----
BILL OF STEEL	M-431-53A	-----

<u>LIST OF STANDARD DRAWINGS</u>	<u>DWG. NO.</u>	<u>REV. DATE</u>
BRIDGE RAILING CONCRETE PARAPET	STD-1-1	7-31-00
REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS	STD-1-5	7-31-00
BRIDGE END DRAIN DETAILS 2'x8'-7" & 4'x8'-7" WITH PAVEMENT AT BRIDGE ENDS	STD-1-6	4-28-97
BRIDGE END DRAIN DETAILS 2'x8'-7" & 4'x8'-7" WITH PAVEMENT AT BRIDGE ENDS	STD-1-7	7-31-00
BRIDGE END DRAIN DETAILS 4'x8'-7" WITH PAVEMENT AT BRIDGE ENDS	STD-1-9	5-1-95
STANDARD PILE DETAILS	STD-5-1	10-25-93
STANDARD PILE DETAILS	STD-5-2	5-21-99
STANDARD SEISMIC DETAILS	STD-6-1	5-21-99
STANDARD SEISMIC DETAILS	STD-6-2	11-07-94
REINF. BAR SUPPORT DETAILS FOR CONC. SLABS	STD-9-1	12-19-94
MISCELLANEOUS ABUTMENT & DRAINAGE DETAILS	STD-10-1	5-11-92
STD. DETAILS AND INT. DIAPH. DETAILS FOR BULB-TEE BEAMS	STD-14-1	7-31-00
SLOPE PROTECTION	RD-SA-1	5-27-98

2020 ADT = 14480
86'-0" ROADWAY W/STD-1-1 PARAPET
DESIGN SPEED = 60 MPH

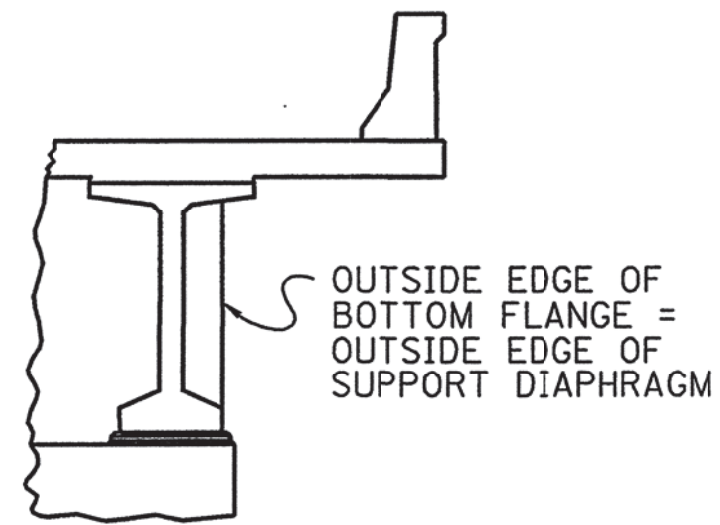
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 2
LAYOUT OF BRIDGE
STATE ROUTE 32 OVER
EXISTING STATE ROUTE 32
BRIDGE I.D. NO.
STATION 423+49.21
CLAIBORNE COUNTY
2002



DESIGNED BY MICHAEL BELL DATE 3-00
DRAWN BY MICHAEL BELL (PMM) DATE 4-02
SUPERVISED BY FIELDS/DIETERS DATE 4-02
CHECKED BY C. DIETERS DATE 5-02

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES



DETAIL "A"

NOTE: NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED. ALSO, SEE STANDARD DRAWING STD-1-1.

NOTE: THE CONTRACTOR IS SOLELY RESPONSIBLE FOR SUPPORTING THE BEAMS TO PREVENT DAMAGE DUE TO TWISTING OR OVERTURNING DURING ALL PHASES OF CONSTRUCTION. IT IS STRONGLY RECOMMENDED THAT THE TEMPORARY ERECTION DIAPHRAGMS BE INSTALLED AND THE PERMANENT INTERMEDIATE DIAPHRAGMS BE POURED AND CURED PRIOR TO PLACING ANY LOADS ON THE GIRDERS HOWEVER, TEMPORARY ERECTION DIAPHRAGMS AND PERMANENT INTERMEDIATE DIAPHRAGMS MUST BE IN PLACE IN THE SPAN AT THE TIME THE SLAB IS POURED IN SAID SPAN.

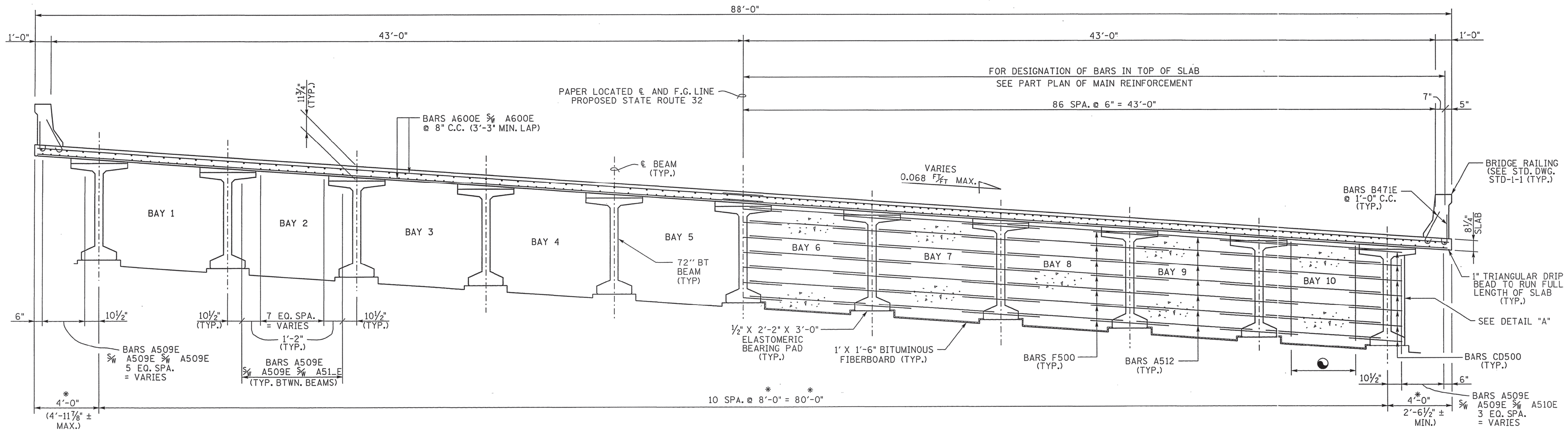
NOTE: THE SUPPORT DIAPHRAGMS AT THE BENTS SHALL BE FORMED AND THE BOTTOM 15 INCHES POURED AS SOON AS POSSIBLE AFTER THE BEAMS HAVE BEEN SET. THE REMAINDER OF THE DIAPHRAGM SHALL BE POURED CONCURRENTLY WITH THE DECK SLAB. ALL DIAPHRAGMS CONCRETE SHALL BE INCLUDED IN THE QUANTITY FOR ITEM 604-03.09.

NOTE: OUTSIDE EDGE OF SLAB AND BRIDGE RAIL TO CONFORM TO
HORIZONTAL CURVE.

CONST. NO. 13001-3227-64

PROJECT NO.	YEAR	SHEET NO.
APD-NH-32(18)	2002	

REVISIONS

[illegible]

HALF-SECTION NEAR MIDSPAN

TYPICAL CROSS SECTION

(LOOKING FORWARD ON SURVEY)

HALF-SECTION NEAR BENT

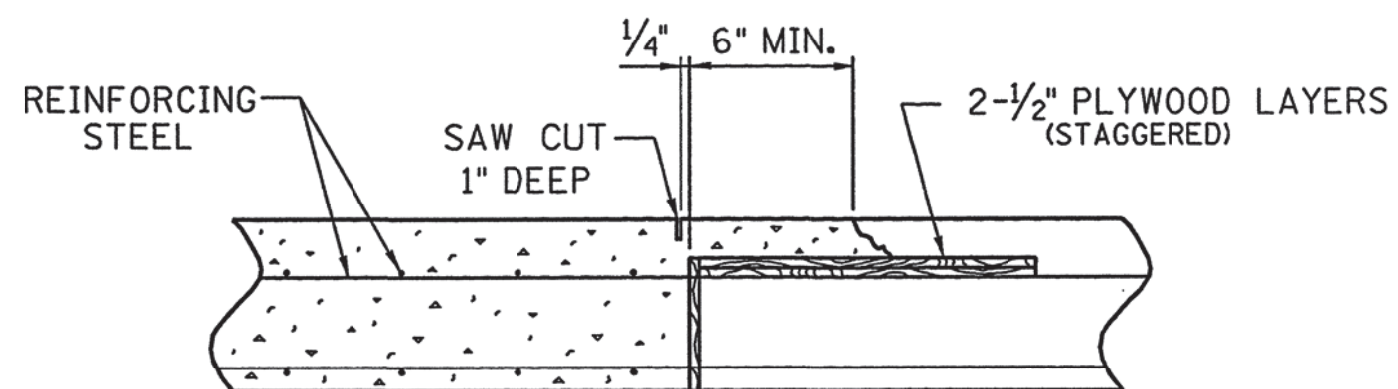
● DENOTES: BARS L400 15 EQ. SPA. = VARIES
(TYP. BETWEEN BEAMS)

* DENOTES: DIMENSIONS ARE FOR
CONCENTRIC ARC

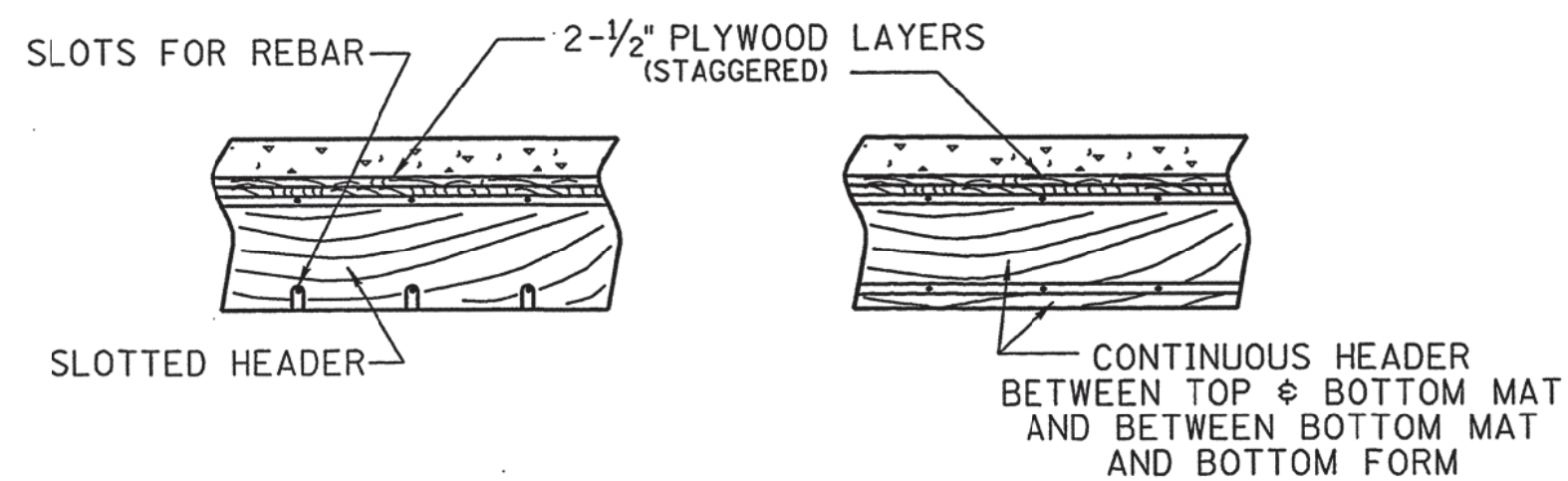
NOTE: BEAMS ARE PLACED AS CHORDS
TO CONCENTRIC ARCS. ALL
SUBSTRUCTURES ARE PARALLEL.

DECK CONCRETE POURING SEQUENCE: SLAB CONSTRUCTION JOINTS MAY BE LOCATED AT THE CONTRACTOR'S OPTION SUBJECT TO THE FOLLOWING:

1. NO CONSTRUCTION JOINT MAY BE LOCATED CLOSER THAN 10 FEET OR FURTHER THAN 15 FEET FROM AN INTERIOR SUPPORT.
2. THE SLAB IN THE MIDDLE SECTION OF BOTH ADJACENT SPANS MUST BE POURED TO WITHIN AT LEAST 15 FEET OF THE SUPPORTS EITHER PRIOR TO OR CONCURRENTLY WITH THE SLAB OVER AN INTERIOR SUPPORT.
3. ALL SLAB CONSTRUCTION JOINTS SHALL BE IN ACCORDANCE WITH THE SLAB CONSTRUCTION JOINT DETAIL SHOWN BELOW.



SLAB CONSTRUCTION JOINT DETAIL



ALTERNATE "A"

ALTERNATE "B"

ALTERNATE HEADER DETAILS



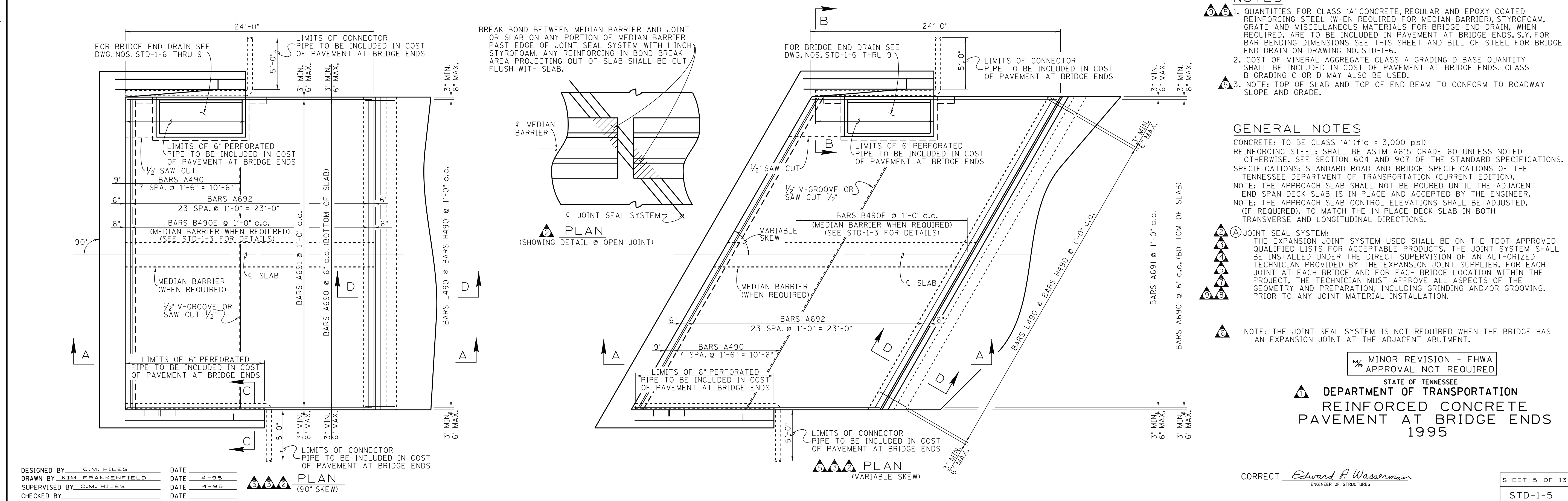
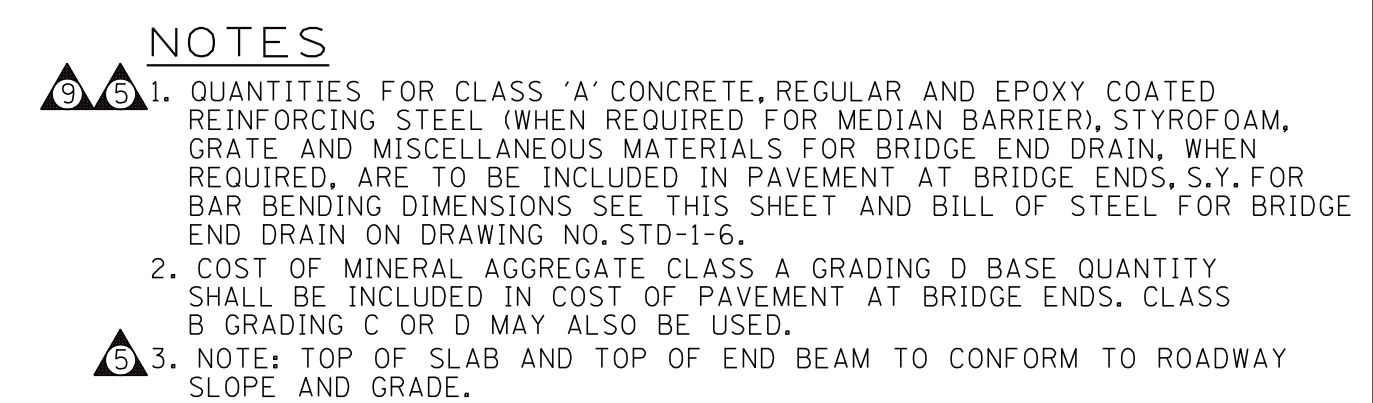
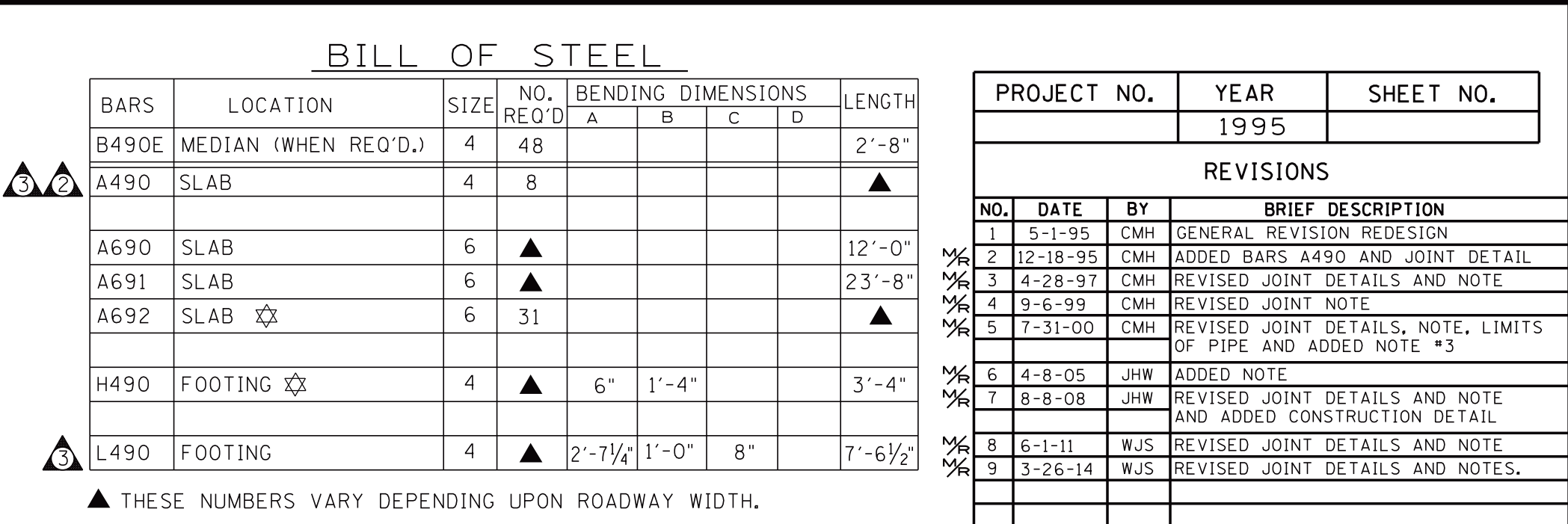
ESTIMATED QUANTITIES

CONCRETE CLASS "D" (BRIDGE DECK) C.Y.	EPOXY COATED REINFORCING STEEL LB.	REINFORC- ING STEEL L.B.
1,068	298,205	11,206

STATE OF TENNESSEE
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STATE ROUTE 32 OVER
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2002

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ENGINEER OF STRUCTURES



GENERAL NOTES

CONCRETE: TO BE CLASS 'A' ($f'c = 3,000$ psi)

REINFORCING STEEL: SHALL BE ASTM A615 GRADE 60 UNLESS NOTED OTHERWISE. SEE SECTION 604 AND 907 OF THE STANDARD SPECIFICATIONS.

SPECIFICATIONS: STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION (CURRENT EDITION).

NOTE: THE APPROACH SLAB SHALL NOT BE POURED UNTIL THE ADJACENT END PAVEMENT SLAB IS IN PLACE AND ACCEPTED BY THE ENGINEER.

NOTE: THE APPROACH SLAB CONTROL ELEVATIONS SHALL BE ADJUSTED, (IF REQUIRED), TO MATCH THE IN PLACE DECK SLAB IN BOTH TRANSVERSE AND LONGITUDINAL DIRECTIONS.

① JOINT SEAL SYSTEM:

THE EXPANSION JOINT SYSTEM USED SHALL BE ON THE TOOT APPROVED QUALIFIED MANUFACTURER ACCEPTABLE PRODUCTS. THE JOINT SYSTEM SHALL BE INSTALLED UNDER THE DIRECT SUPERVISION OF AN AUTHORIZED TECHNICIAN PROVIDED BY THE EXPANSION JOINT SUPPLIER, FOR EACH JOINT AT EACH BRIDGE AND FOR EACH BRIDGE LOCATION WITHIN THE PROJECT. THE TECHNICIAN MUST APPROVE ALL ASPECTS OF THE GEOMETRY AND PREPARATION, INCLUDING GRINDING AND/OR GROOVING, PRIOR TO ANY JOINT MATERIAL INSTALLATION.

NOTE: THE JOINT SEAL SYSTEM IS NOT REQUIRED WHEN THE BRIDGE HAS AN EXPANSION JOINT AT THE ADJACENT ABUTMENT.

M/R MINOR REVISION - FHWA
 APPROVAL NOT REQUIRED

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
REINFORCED CONCRETE
PAVEMENT AT BRIDGE ENDS
1995

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

SHEET 5 OF 13
STD-1-5